

Moolelo Kahiko Helu Ekahi

Tradition of Excellence



40th Anniversary
Hawaii Air National Guard

Dedication

This book is dedicated to the men and women of the Hawaii Air National Guard for their commitment to the mission and to the family, friends and civilian employers who, in their support of Guard members, are also making an essential contribution to the defense of the State of Hawaii and the nation.

FOREWORD



As we celebrate the 40th Anniversary of the Hawaii Air National Guard, we can look back at a history filled with many accomplishments. These accomplishments were not achieved through chance, but by sacrifices and dedication of many people who have worked hard to make the Hawaii Air National Guard an outstanding organization. The "can do" attitude of our people that has prevailed throughout the years has helped us to overcome adversities and successfully meet each challenge. The tremendous sense of pride of our people has made us a highly professional and combat ready organization.

Our accomplishments have been recognized by many awards from the National Guard Bureau and the United States Air Force. We have fulfilled whatever task or mission that has been assigned to our units and we have become a truly active partner in the defense of our Nation.

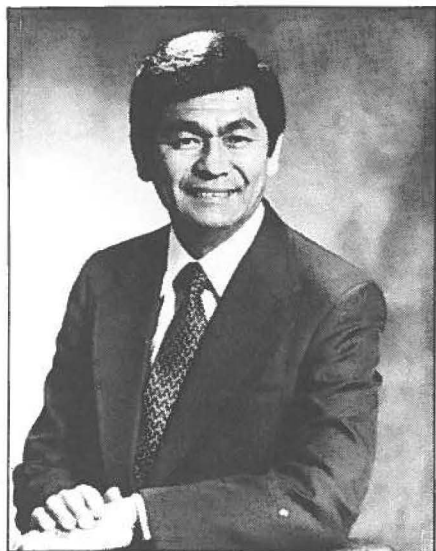
In honor of the past and present members of the Hawaii Air National Guard who have contributed so much to our organization, we celebrated this 40th Anniversary.

In addition, this year marks, several other noteworthy events. This is the 30th Anniversary of the Hawaii Air National Guard's air defense mission for the State of Hawaii. The 150th Aircraft Control and Warning Squadron celebrates their 25th Anniversary this year and we also are celebrating the 350th Anniversary of the National Guard.

To all present and former members of the Hawaii Air National Guard, I wish to express my heartfelt thanks for a job well done and wish all of you an enjoyable 40th Anniversary.

Edward V. Richardson

**EDWARD V. RICHARDSON, Brig Gen, HANG
Commander**



A Letter from the Governor



EXECUTIVE CHAMBERS
HONOLULU

GEORGE R. ARIYOSHI
GOVERNOR

MESSAGE FROM GOVERNOR GEORGE R. ARIYOSHI TO THE MEN AND WOMEN OF THE HAWAII AIR NATIONAL GUARD

I salute you for the 40 years of meritorious service to our state and nation. You represent a dedicated corps of men and women who have devoted countless hours in providing community service and support. You have had the unique responsibility of providing the air defense for the Hawaiian Islands for more than 30 years and have excelled in performing many other duties. You can take great pride in your many accomplishments, and I commend all who are serving and have served in the Hawaii Air National Guard. All of us are grateful for what you have done.

George R. Ariyoshi
George R. Ariyoshi



A Letter from the State Adjutant General



STATE OF HAWAII
DEPARTMENT OF DEFENSE
OFFICE OF THE ADJUTANT GENERAL
3949 DIAMOND HEAD ROAD, HONOLULU, HAWAII 96816-4495

GEORGE R. ARIYOSHI
GOVERNOR

ALEXIS T. LUM
MAJOR GENERAL
ADJUTANT GENERAL
PAUL K. NAKAMURA
COLONEL (H1)
DEPUTY ADJUTANT GENERAL

TO THE MEN AND WOMEN OF THE HAWAII AIR NATIONAL GUARD

The Hawaii Air National Guard (HANG) has a proud and distinguished record of service to the community, state and nation. Since its inception in 1946, the HANG has consistently exceeded high standards of readiness and operational excellence through training, equipment modernization and planning. Today, the HANG continues its vital mission as the only reserve unit in the nation charged with the military defense of a given area. The pioneering efforts, foresight and professionalism of the HANG's people, past and present, have contributed to the increased capabilities and accomplishments of all HANG units. The pride and devotion to duty each of you display reaffirm the viability of expanding the role of reserve/national guard forces in military defense strategy in the Pacific Region and the rest of the Free World. On this, the 40th Anniversary of the Hawaii Air National Guard, I extend to each of you my sincere appreciation for the outstanding contributions, service and selfless sacrifices you have made in building a strong and ready force dedicated to the preservation of peace and freedom.

Alexis T. Lum
ALEXIS T. LUM
Major General, HANG
Adjutant General



MOOLELO KAHIKO HELU EKAHI
A Tradition of Excellence

was

compiled by:

Lt. Col. Jerald K. Aoki, Chairman
 HANG 40 Committee

With Research and Development by:

Lt. Col. Warren W.K. Chung
 Maj. Bruce K. Minato
 Capt. Galen R. Yoshimoto
 CMSgt. Darryl D.J.H. Ho
 MSgt. Iris T. Kabazawa
 TSgt. Fred M. Higa, Jr.
 SSgt. Iris D. Torres
 TSgt. Charles S. Gravitt

Editor:

Chris Casey

Design & Layout:

Raymond Santana
 RS Graphics

Cover Photo by:

Ronald S. Biho

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UNIT LOCATIONS

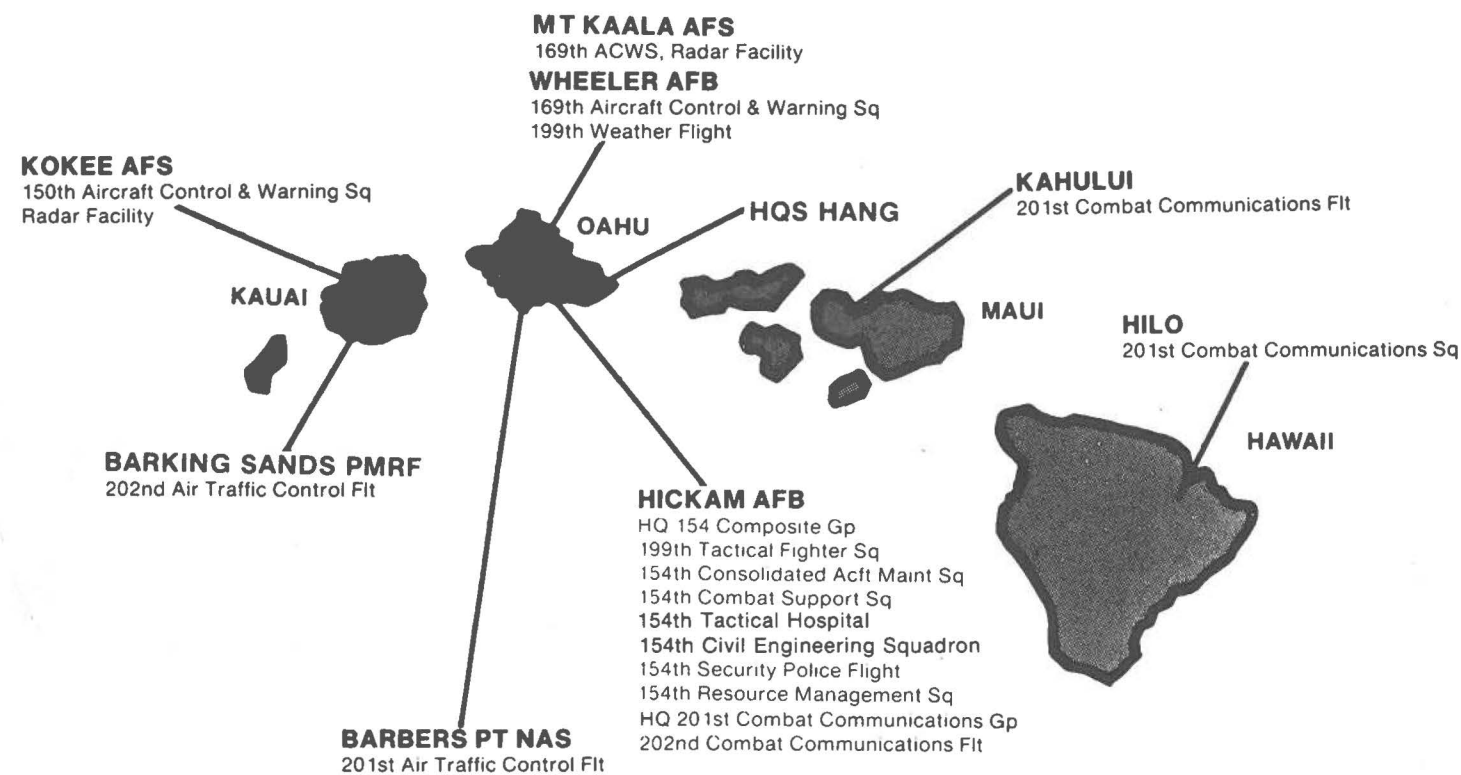


TABLE OF CONTENTS

THE FIRST THIRTY YEARS page 6
 COMMANDERS PAST AND PRESENT page 23
 HANG AIRCRAFT HISTORY page 24
 ORGANIZATIONAL CHART page 25

1976—1986

HQ HANG page 26
 154th COMPOSITE GROUP page 29
 199th TACTICAL FIGHTER SQUADRON page 31
 154th CONSOLIDATED AIRCRAFT MAINTENANCE SQUADRON page 34
 154th RESOURCE MANAGEMENT SQUADRON page 37
 154th COMBAT SUPPORT SQUADRON page 39
 154th CIVIL ENGINEERING SQUADRON page 41
 169th AIRCRAFT CONTROL AND WARNING SQUADRON page 43
 150th AIRCRAFT CONTROL AND WARNING SQUADRON page 45
 154th TACTICAL HOSPITAL page 47
 154th SECURITY POLICE FLIGHT page 49
 199th WEATHER FLIGHT page 51
 201st COMBAT COMMUNICATIONS GROUP page 53
 201st COMBAT COMMUNICATIONS SQUADRON page 55
 201st COMBAT COMMUNICATIONS FLIGHT page 57
 202nd COMBAT COMMUNICATIONS FLIGHT page 59
 201st AIR TRAFFIC CONTROL FLIGHT page 61
 202nd AIR TRAFFIC CONTROL FLIGHT page 63
 WE ARE THE FAMILY OF HANG page 64
 FAMILY DAY '86 page 65
 PEOPLE PROGRAMS page 68
 HANG ATHLETICS page 69
 MEMORIAM page 70



Happy 40th Anniversary HANG from the pilots of the 199th TFS.

HAWAII AIR NATIONAL GUARD 40th ANNIVERSARY The First Thirty...

World War II was over and the troops had come home. Summer was drawing to a close. Hawaii Governor Ingram M. Stainback ordered the Hawaii National Guard reestablished in August under the command of the new Adjutant General, Brig. Gen. Fred W. Makinney. Among these were four Army Air Force units: the 199th Fighter Squadron; the 199th Utility Flight; the 199th Air Service Group, Detachment C; and the 199th Weather Station.

These Army Air Force units, new to the Hawaii National Guard, were organized Sept. 10, 1946 by the Military Department, Territory of Hawaii, under General Order No. 4, with temporary station at the Honolulu Armory.

The first commanding officer of the 199th Fighter Squadron was Lt. Col. Walter H. Dillingham, who resigned to run for the Territorial Legislature. Colonel Dillingham was followed by Maj. William B. Morgan and then Captain Frank R. Harlocker took the helm.

By the end of October enough people had been recruited to meet the minimum requirements for federal recognition. There were seventy-four names on the rolls and in the ranks when the units were inspected at 7:30 p.m., Nov. 4, 1946, by an Army Air Force Inspection Team. Capt. Valentine A. Siefermann, then on active duty awaiting separation, was also present.

Maj. William F. Nolan took command of the 199th Fighter Squadron in December and by the end of that first year a por-

tion of Bellows Field was made available to the air units. The area included two main runways, one Butler hanger, three large warehouses, an administration building, a parachute building and a motor pool. Former quarters were also made available and some of the 56 full time employees moved in.

The HANG received its first aircraft, a B-26C, no. 433, on Mar. 19, 1947. By the end of the fiscal year nine more aircraft had been added to the inventory: two AT-6s, four B-26s, one C-46 and two L-5s. There were, as yet, no tactical aircraft assigned.

Parts were hard to come by necessitating the need for "scrounging" raids to Hickam Field by a group known as "Nolan and his Forty Thieves."

The Control Tower was only manned during known flying schedules. Occasionally it was unmanned during "unknown" flying schedules.

But Bellows Field was not considered the most feasible site for the Hawaii Air Guard because of its geographical location. The roads were not as good as they are today, making it difficult for members to attend drills. A lot of time was lost flying back and forth to Hickam Field for supplies. And looking far, far ahead Bellows was deemed undesirable for future jet operations. By May of 1947, we began looking for a new home.

Jubilation was the order of the day, July 9, 1947, when the first JUG, a P-47 Thunderbolt, no. 529, touched down joining the Guard's inventory.



SOME OF THE FIRST HANG MEMBERS AT BELLOW'S FIELD, Nov. 23, 1946. *Seated:* 1st Lt. George R. Duncan, 1st Lt. Walter F. Judd, 1st Lt. John L. Perry, unknown, 1st Lt. David L. Withington, 1st Lt. Russell L. Smith, Capt. Charles C. Royce, Capt. Walter J. Kirschke, 1st Lt. Alfred D. Shaheen, 1st Lt. Edward L. Stuart, Capt. William D. Weichert, 1st Lt. Robert M. Barry, Capt. John M. Becker, unknown, Lt. Alexander R. Anderson, Maj. Leonard S. Marshall (SAI). *Kneeling:* SSgt. Donald I. Carpenter, (?) M.S. Padeken, SSgt. Richard S.O. Lee, Sgt. Earl T. Padeken, unknown carpenter, unknown carpenter, Pvt. Richard L. Wong, SSgt. Harold J. Martinez, unknown, TSgt. Winton O. Sanson, SSgt. Oscar H. Weiss, Cpl. Will K. Brown, unknown, *Standing:* Capt. Valentine A. Siefermann, MSgt. Merlin J. Davidson, SSgt. Edward R. Watson, SSgt. Ernest B. Holmwood, Cpl. Charles L. Flanery, (?) Donald Hopkins, Pvt. Simeon Domingo, Cpl. Lemuel L. Jones, Pvt. Richard M. Uchida, SSgt. Herbert Ing, TSgt. Robert A. Kemp, unknown, Pvt. Irving R. Apana, Sgt. Frederick H. Rittmeister, SSgt. Colburn S. Thrum, SSgt. Joseph L. Barnett, (?) W.C. Hill, SSgt. Clydesdale L. LaPointe, SSgt. Sigund B. Jensen, unknown guard, Maj. William F. Nolan (Comdr.).



JUGs!

Two weeks later an inspection by the Hq AAF Inspector General's Office found: "No inventory of property has been completed since activation. There is a lack of sufficient personnel and/or trained personnel to accomplish this within the required periods." And yet... four days later the fledgling Hawaii Air Guard participated in its first air show and open house at Hickam Field as part of the 40th Anniversary of the Army Air Force. Local news media reported: "In rapid succession the speedy P47 fighters simulated strafing attacks on the air field..."

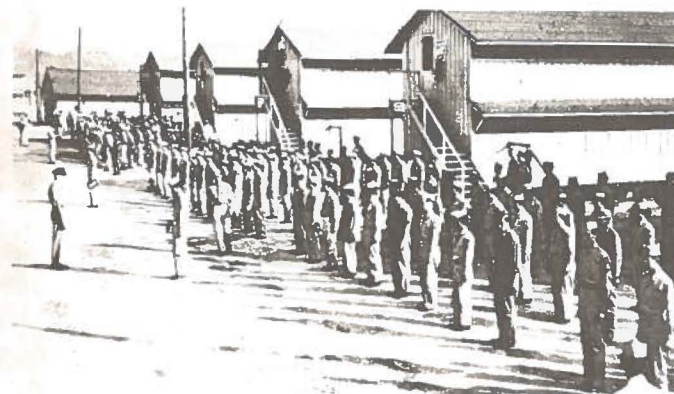
By September of that year, Hickam Field became our official home, under orders from 7th Air Force. The move was completed by October. The old 19th Troop Carrier Squadron area was assigned to the HANG and included: two open-ended Butler hangars and a miscellaneous group of wooden shacks left over from World War II.

More aircraft were added and celebration came again on Sept. 18, 1947, when the Hawaii Air National Guard became a reserve component of the newly constituted United States Air Force. Although still rough around the edges, the HANG was on the road to becoming a real fighting force.

About this time, Hawaii Air Guard leaders established a goal: To be the outstanding Air National Guard unit with a combat effectiveness rating second to none. Those in the know shook their heads—these would be blood, sweat and tears times. They were.

The Hawaii Air National Guard grew from a cadre of World War II veterans and a pool of inexperienced recruits to a fully qualified tactical combat operation. Aggressive tactical flying training became the keynote under the guidance of the new Operations Officer, Maj. Lucien E. Parrish, Jr. Flying training was upgraded from single engine transition and "boring holes in the sky" to four ship flights engaged in aerial gunnery, ground strafing, rocket firing and dive bombing.

Dawn Patrols began Nov. 14, 1948. Pilots took off at 5:50 a.m. and landed at 7:15 a.m., just in time to go to work at their civilian jobs. There were also daily Twilight Patrols, after normal working hours. This aggressive flying schedule produced 4,462 flying hours during the first year.



ROLL CALL. Summer Camp formation, Area 61, Hickam AFB.

HANG 1986

Maj. Lucien Parrish assumed command of the 199th Fighter Squadron on Sept. 8, 1949 and two weeks later the HANG showed its "stuff" in Operation MIKI. MIKI was the code name for a massive joint task force exercise which involved some 4,000 military and naval personnel and over 125 naval ships. A Western Task Force was formed in California, composed of some 100 vessels and 18,000 Army Troops, missioned to invade Oahu.

As the exercise "D-Day" approached, several "enemy submarines" were sighted and attacked by HANG F-47s (the P-47s had been redesignated "F" for fighter) and B-26s. HANGmen went on drill status the 22nd and 23rd of October—on the 22nd there extensive patrols and on the 23rd the aerial HANGmen hit the jackpot!

Fourteen F-47s and three B-26s took off from Hickam AFB at 11:00 a.m. on Oct. 23 and headed for the island of Hawaii. One JUG developed engine trouble about 30 minutes out and his wing man escorted the unhappy pilot home.

The formation flew down the island chain at 6,000 feet and passed on the leeward side of Mauna Loa, using the mountain as radar cover. When the formation reached South Point they headed 120 degrees and descended to 3,000 feet countermeasures began—"window", "rope", and snow."

A force of two carriers, two cruisers and six destroyers was sighted 75 miles out, and the unexpected HANG aircraft attacked with "bombs", "rockets", and "strafing." The F-47s made three passes, the B-26s made two. Red Flight reported ten landing carrier aircraft "destroyed", Blue Flight reported five F-4Us "destroyed, with one "loss", Yellow Flight estimated eight enemy aircraft "splashed, and Aztec Flight (B-26s) bombed away like crazy (under attack). The aerial surprise attack was a complete success.

The National Guard Bureau announced in 1950 that the 199th Fighter Squadron had won one of the twelve Spaatz trophies for outstanding capabilities in 1949, based on the exceptionally high O.R.T. (Operations Readiness Test) score of that year. In fact, the 199th scored highest of all 84 Air National Guard Squadrons of the 12 Wings.

The Military Department of the Territory of Hawaii moved from Honolulu Armory to Fort Ruger in 1950. And in December of that year, the 109th Aircraft Control and Warning Squadron (109 ACWS) was organized. Maj. Valentine Siefermann was Commander at Federal Recognition.

Equipment was scarce so the emphasis was on General Military Training until equipment could be "acquired" in the tradition of "Nolan and his Forty Thieves." The first meeting was held in the corner of the hangars.

Maj. Frank R. Harlocker assumed command of the 109th ACWS in April 1951; Maj. Siefermann went to Fort Ruger and became Air Staff Officer on the Adjutant General's staff replacing Lt. Col. John Piper. Maj. James R. Ashford became Operations Officer of the 109th ACWS and Capt. Kiyoshi P. Goya took over the position of Radar Station Supervisor.

Maj. Carlos W. Bonham assumed command of the 199th Fighter Squadron, May 25, just after he won permanent retention of the Brady Trophy for aerial gunnery (named for Lt. Leopold J. Brady, who was killed two years earlier when his



FAREWELL BONNY. Barking Sands AFB, June 24, 1952.

JUG crashed on Hickam's Runway 7 upon returning from a morning aerial gunnery mission.)

The fighter squadron held its summer camp at Barking Sands, Kauai, June 9-24, 1951. A1C Arthur Lee cracked up a 2 ton truck returning from pass run to Lihue. This was the first incident of this kind and a number of the HANGmen became "Legal Beagles" trying to set up the correct paperwork. MSgts. Carl Kang and Harold Mattos (among others) returned late from pass and were restricted to base for two days (MSgt. Robert Choi earlier escaped the same fate by faking sick.)

Ironically, Maj. Bonham was killed the following year, June 15, 1952, in a midair collision while leading a flight of JUGs in acrobatics in-trail. The other pilot parachuted to safety in the sea between Barking Sands and the island of Niihau.

The 199th Fighter Squadron went through two name changes in 1952—from the 199th Fighter Squadron to the 199th Fighter Bomber Squadron on July 15 to the 199th Fighter Interceptor Squadron (199 FIS) on Nov. 19. The 109th AC&W Squadron moved from its home at Battery Closson at Fort Kamehameha to the Kokohead Crater Radar Station.

Maj. James R. Ashford assumed command of the 109th on Nov. 19. Capt. Kiyoshi P. Goya became the new C.E. Staff Officer.

The 199th Weather Station was reorganized and redesignated the 199th Forecasting Flight on May 1, 1953. Capt. Allen D. Pearson joined the HANG and proved to be a most vigorous weather commander. He became known as "Big Stormy".

Fiscal Year (FY) 1954 started off with a bang! The word came down that the World War II vintage JUGs were to be replaced with jets—North American F-86E SABRE JETS of recent Korean War fame, and several dual-place jet trainers, Lockheed T-33 T-BIRDS. Supply was the first to swing into action. It had to determine what was authorized, submit requisitions and prepare to warehouse over 6,000 line items for the T-BIRDS and for the SABRE JETS.

A new unit was formed—the 8199th Replacement Training Squadron—accelerating the program of recruiting the best of Hawaii's young men for USAF-conducted pilot training and subsequent return to the HANG.

The tempo of preparations for the forthcoming jets picked up. Ten Air Technicians, headed by MSgt. Robert D.W. Choi (Line Chief) and MSgt. Harold T. Mattos (Chief Inspector), left in October to attend Jet Mechanic School in Amarillo, Texas. Maj. Archibald M. Harrison, Squadron Commander, and five Air Technician mechanics attended a 30 day F-86E Mobile Training Detachment course at Gowen AFB, Idaho in January of 1954.

The first T-33 (No. 273) arrived Nov. 12, 1953. The first SABRE JETS came in Feb. 10, 1954 and were greeted by a large, enthusiastic group of HANG members. There were many willing hands to "de-cocoon" them of their sprayed waterproof protective coverings. TSgt. Simanu Afoa, Jr., a

Korean veteran, recognized F-86E No. 740 as having been flown by Maj. Jabara, the first American Ace of the Korean War.

Aside from the monumental task of training Hickam HANGmen in jet-age equipment, logistics were a real headache. Ground handling equipment and parts filtered in only too slowly—some needed items were made by TSgt. Frederick H. Rittmeister, the HANG machinist, so that work on the aircraft could be accomplished.

After all the fighters had been checked out, the 199th Fighter Interceptor Squadron invited the public to share their pride and joy of having one of the latest type of jet interceptor aircraft. A full scale Air Show was held at Honolulu Airport in February 1954. Miss Sabre Jet was quite an attractive attraction.

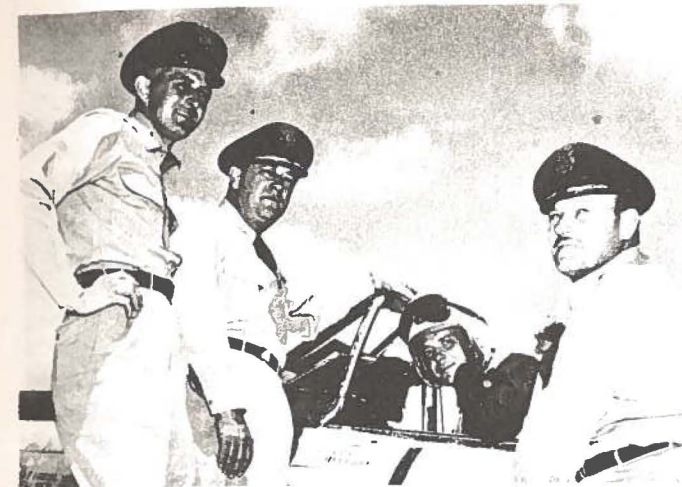
The 109th AC&W Sq. held end-to-end summer encampments at Koko Crater AFS. Training was on a 24-hour basis (they now had a height finder). The training program emphasized jet transition, F-86E formation flying, familiarization aerial gunnery, and ground control interception missions in conjunction with the 109th.

At the year's end the pilots had logged 4,822 flying hours in the various aircraft. Aircraft maintenance had become proficient enough in jets to support the 415 annual field training sorties with a SABRE JET 93% in-commission rate.

It was early in FY 1955 when Maj. Gen. Sory Smith, Commander PACAF (Rear), made a startling proposal to the



PILOTS IN FRONT OF DOUGLAS B-26. *Squatting:* 2nd Lt. Hunter C. Parker, 2nd Lt. Leopold J. Brady, Capt. Archibald M. Harrison, 1st Lt. James F. Coleman, 2nd Lt. Frank J. Smith, 2nd Lt. Tyler M. Harr, 1st Lt. Jarrett F. Carr. *Standing:* 1st Lt. Carlos W. Bonham, Capt. Alfred D. Shaheen, 2nd Lt. Letwell P.A. Duvauchelle, 1st Lt. George R. Duncan, Capt. Douglas E. Guy, Maj. Valentine A. Siefertmann, Maj. Frank R. Harlocker, Lt. Col. William F. Nolan, 1st Lt. Robert B. Maguire, 2nd Lt. George T. Harris.

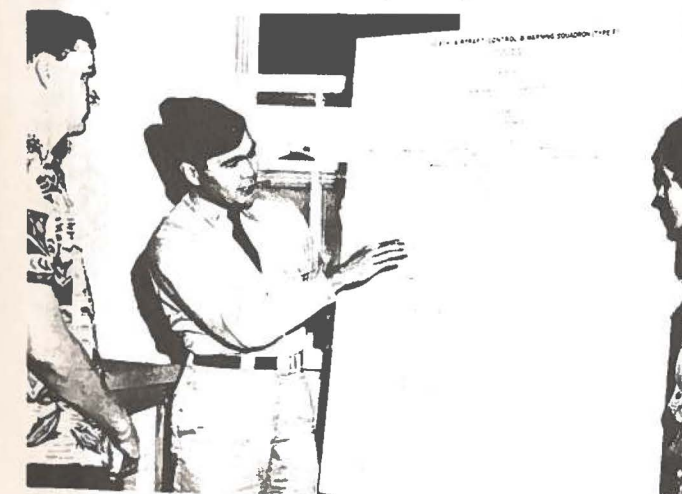


NGB VISITORS. Maj. Valentine A. Siefertmann, Maj. Gen. Earl T. Ricks, Capt. Robert G. O'Hara and Col. Winston P. Wilson.

Hawaii Air Guard. As there was no active air defense of Hawaii, he proposed that the HANG provide fighters on alert at Hickam—similar to the air defense augmentation programs of Air National Guard fighter units in the continental United States—except that the HANG would provide the total air defense capability of the Hawaiian Islands geographic area. Further, that the 109th AC&W Sq. provide ground air defense control capabilities, the first active support mission of this type for the Air National Guard. The proposal was accepted with enthusiasm by HANG leaders. USAF approved, and the HANG began planning and coordinating with the National Guard Bureau. Efforts for operational capabilities in the new SABRE JETS increased markedly.

The 199th and 8199th squadrons concentrated on qualifying in the high performance day-fighter SABRE JETS. Fighter pilots continued the "Dawn Patrols" of JUG fame and flew round-robins to Maui. Take-off was at 5:30 a.m. and landing was in time to get to work. The 199th and 8199th began to hold separate Sunday drills to get better utilization of the 15 SABRE JETS.

Eighteen days of special aerial exercises were held January-March 1955. Each of the 29 pilots was programmed for six days. Some 340 sorties were flown, over 48,000 rounds of cal. .50 were expended. A flight of four would be in the aerial gunnery pattern; tips of the cal. .50 bullets were painted a different color in each aircraft for scoring purposes. The T-BIRD replaced the TB-26 for towing targets. Three fighter pilots "broke the ice" and became the first to qualify both below and above 20,000 feet: Maj. George R. Duncan and 1st



IT'S LIKE THIS. Capt. Kiyoshi P. Goya explaining the 109th AC&W Sq. organizational structure to new recruits.

Lt. Kenneth L. Fisher qualified as Sharpshooters, 2nd Lt. John E. Russell as Marksman.

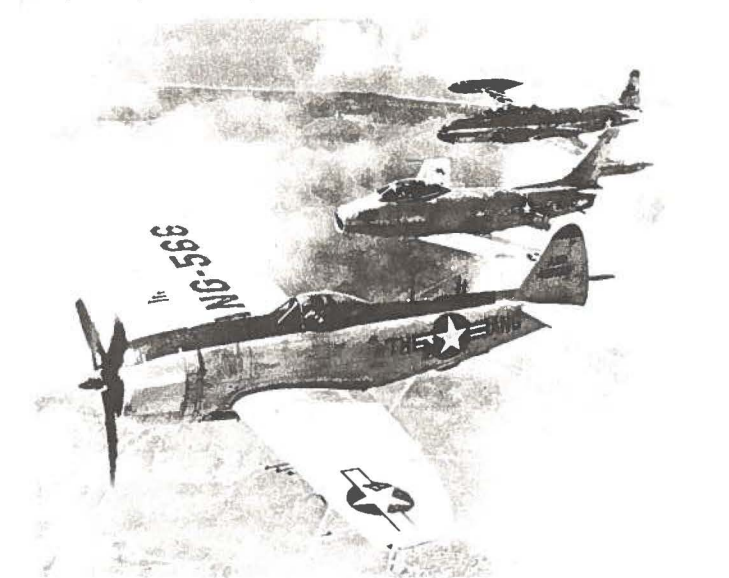
Cpts. Jarret F. Carr and Letwell P.A. Duvauchelle, 2nd Lts. Robert A. Hinkley and John L. Russell flew an air race "Against the Clock" from Hilo to Honolulu as part of Armed Forces Day. Lt. Russell won the North American Aviation Co. Trophy in SABRE JET No. 676: time 19 minutes (he flew at 6,000 feet.)

Lt. Col. James R. Ashford assumed command of the 199th FIS on Feb. 15, 1955. Maj. Robert E. Paine assumed command of the 109th AC&W Sq. the same date.

Maj. George R. Duncan assumed command of the 8199th Replacement Training Sq. July 1, 1955. Capt. Letwell P.A. Duvauchelle became Operations Officer.

Maj. Robert B. Maguire was presented the 7th Air Force Gunnery Trophy for high aerial gunner score (20.3%) during the previous month's summer camp competition. Lt. Paul L. Hummel was second (18.5% hits) and Lt. John E. Russell was third (18.1%).

Lt. Col. James R. Ashford, Maj. Robert Maguire, Lts. Robert A. Hinkley, Paul L. Hummel, and John E. Russell represented the HANG at the Air National Guard Gunnery Meet, Sept. 8-17, 1955, at Boise, Idaho. F-86s were borrowed from the



THE OLD AND THE NEW. Mar. 7, 1954. JUG: Capt. William W. Dau. SABRE JET: Maj. Archibald M. Harrison. T-BIRD: Lt. Col. Frank R. Harlocker and Col. Barry B. Townsend.

Phoenix, Arizona Sq. Bad weather, delays of all sorts, and cal. .50 rounds cooking off on the ramp, voided the gunnery meet.

The 109th AC&W acquired a second home in November 1955, the former Federal Quarantine Station at Sand Island which had been used as a Prisoner of War compound during World War II. There were ample training facilities (a dozen buildings) which were rehabilitated. A Butler hangar was erected for supply. A temperate tower and a AN/FPS-8 search operational facility remained at Koko Crater AFS.

HANGmen blithely decided to make a 28 minute 16 mm color movie with sound about the fighter squadron! No funds could be secured, and the production of Aerial Minuteman proved to be the biggest "scrounge job" ever accomplished by HANGmen. A brash HANG man sat down and wrote his first script. Cooperative technical capability was "discovered" within the active establishment (Army) and a trained cameraman became "available". Film "appeared" from many, many sources. The squadron "stars" began to watch their haircuts. Absolutely hair-raising techniques were developed to get some splendid shots of aerial gunnery. The movie took a year to "put into the can" and was meshed with normal training and flying schedules. HANGmen completed as much as they could—the National Guard Bureau provided funds for the Department of Agriculture to assemble the ori-

ginal kodachrome and prepare a composite soundtrack. AERIAL MINUTEMEN proved a smash hit (to the HANG men)-it was excellent for recruiting, orientation and pride in the HANG.

The tempo of the HANG was dramatically accelerated in February 1956. Approval was received for the HANG to actively participate in the active air defense program effective July 1, 1956. The 199th and 8199th were to provide two combat ready pilots and fully armed SABRE JETs on five minute Alert during daylight hours. The 109th was to provide radar surveillance and ground controlled intercept (GCI) capability-at the deactivated Punamano AFS!

AC&W workers began a crash program. The tower erected at Sand Island was hurriedly dismantled and re-erected at Punamano under the leadership of Capt. Arthur U. Ishimoto. MSgts. Susumu Furuike and Charles Singletary. Seventh Air Force rehabilitated the existent structures at Punamano-a small operations building, a generator shed, a latrine, and a small supply building. A new air/ground radio building was constructed.

Maj. Robert G. O'Hara assumed command of the multi-based 109th AC&W Sq. May 31, 1956. Capt. Peter M. Fowler was the Operations Officer and Maj. Kiyoshi P. Goya was the Electronics Officer.

Maj. Gen. Winston P. Wilson, Chief of the Air Division of the National Guard Bureau, visited the HANG during a summer



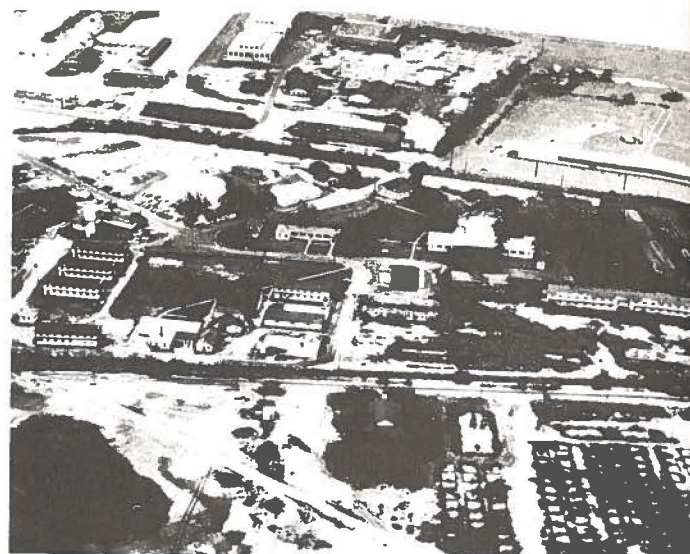
SCRAMBLE! Lts. Paul L. Hummel and Robert A. Hinkley race for their aircraft.

encampment at Kaneohe MCAS in June and observed a remarkable four ship diamond flyby. Capt. Letwell P.A. Duvau-chelle, the Flight Leader, was right on time, but a little fast. One mile out he transmitted a strange message-"speed brakes . . . momentarily." He did just that, the rest of his flight were bewildered at the strange order, and what was a tight diamond disintegrated over the General into what reminded one HANG member of a group of mynah birds who had just been shot at.

Capt. Duvau-chelle, who was thereon referred to as "Momentary Duvau-chelle," explained later that the command had to do with the difference in timing between hydraulic and mechanical air brakes.

Lt. Col. John G. Giraud replaced Col. Barry B. Townsend as Air Force Senior Advisor June 19, 1956. Lt. Col. Giraud's previous assignment had been Commander of the USAF Fighter Weapons School, Nellis AFB, Nevada, and his impact upon the HANG was to be significant.

The Hawaii Air Guard commenced active air defense of Hawaii under the control of 7th Air Force at midnight, July 1, 1956. The Air Defense Control Center (TAILBOARD) was located in Kunia Tunnel, and the Control Center Chief was (active duty) Capt. DeWitt C. Bye.



AC&W HEADQUARTERS. Sand Island facility, headquarters of the 109th AC&W Sq.

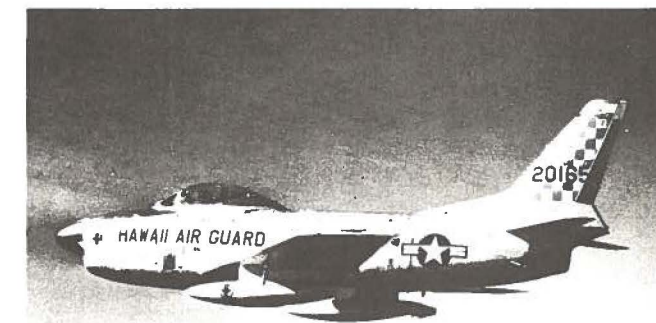
At "zero hour" 1st Lt. Paul S. Mori and his flight of nine operators, one radio technician and two radar technicians of the 109th AC&W Sq. commenced round-the-clock operation of the Punamano Direction Center (TATTOO).

At sunrise that day two operationally ready pilots of the 199th FIS and two fully armed day-fighter SABRE JETs commenced sunrise to sunset five-minute active air defense Alert (WING DING).

An average of 14 fighter pilots went on Alert in short-tour active duty status each month. Five were on active duty at any one time. The Alert aircraft were parked at the head of the flightline closest to the taxiway to Honolulu Airport runway 8. The Alert pilots' "lounge" was a bench outside of the 199 FIS Operations shack. When the 25 cent scramble buzzer sounded (audible radius of 25 feet), the two pilots on duty would make a 100 yard dash to their SABRE JETs. The tower would hear breathless transmissions: "Hickam tower-this-is-White One on a-scramble."

It wasn't long before the Alert pilots and crew chiefs moved to a "li'i ole shack" close to the Alert aircraft, adjacent to "Fort Beaumont." A little later the Alert setup was moved to a site adjacent to Taxiway Bravo (location of the present Alert Pad). Alert facilities grew to two wooden shacks, a pierced-steel-planking ramp, and a horseshoe pitching set.

At Punamano, Capt. Arthur U. Ishimoto was the Air Technician Supervisor and the fulltime complement totalled 54 men. The controllers (Capt. Frederick M. Hemmings, Harold



OFF WE GO! Ls into the wild blue yonder.

K. Lee, Lts. Paul S. Mori, William E. Mossman and Donald O'Day) also provided GCIs for the Marine FJ-2 BANSHEE all-weather fighters. Most of the flying was from 6:30 pm-11:00 pm hours; sometimes they would fly from sundown to sunup. A tremendous number of intercepts were logged-the controllers developed callouses on their elbows and resorted to wearing rubber pads.

Majs. George R. Duncan (Team Capt.), Archibald M. Harrison, Capt. Lorenz L. Kumeilike, Lts. Kurt E. Johnson and Herbert M. Sakamoto comprised the HANG team which competed in the Annual Air Guard Gunnery Meet in Sept. 1956 at Casper, Wyoming. F-86s were borrowed by the HANG from the Phoenix, Arizona ANG. Our team placed eighth, and vehemently stated that the gun harmonizations left much to be desired. They cited the Phoenix Ops briefing as something like this: "This aircraft flies low and ahead, so to hit the target aim the piper four feet high and ten feet back of the bull". Talk about aerial Kentucky windage. . .!

Seventh Air Force conducted its first air defense exercise in Oct. 1956. It lasted some four hours, and the targets were B-29s and T-33s. The "box score" was 100% of the fakers detected, identified, intercepted and . . . destroyed.

The HANG multi-stationed AC&W structure was reorganized Oct. 7, 1956. The 169th AC&W Sq. was formed from a 109th cadre with headquarters at Sand Island and operational facility at Koko Crater AFS. Maj. Kiyoshi P. Goya was appointed Commander, Capt. George T. Miki was Aircraft Controller Staff Officer and Capt. John C.Y. Ing was C-E Staff Officer. The 109th AC&W Sq. was cut down in numbers, and the station was established at Punamano AFS only.

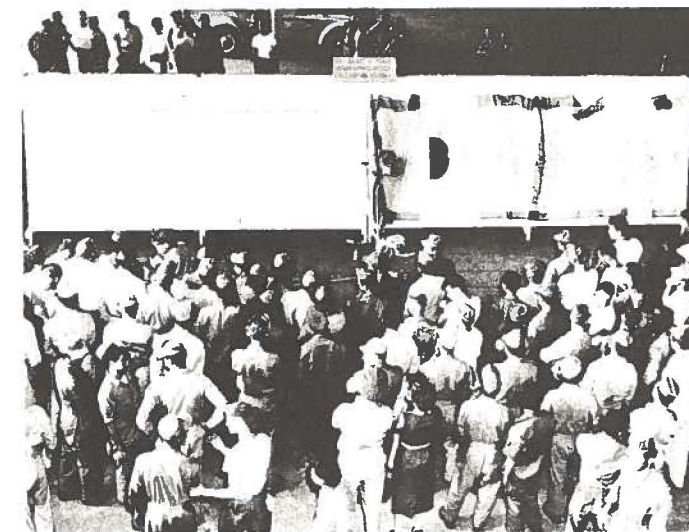
HANG members celebrated their 10th Anniversary with a luau and dance Nov. 3, 1956. Governor Samuel W. King addressed the aloha-attired Hawaii Air Guardsmen. Maj. Walter F. Judd, Master of Ceremonies, presented the 199th history ably assisted by nine others who recounted anecdotes: Capt. Tyler M. Harr, MSgts. William M. Enoka, Sun Yau Goo, Carl S.B. Kang, TSgts. Vernon S. DeLima, George Joaquin, SSgts. George H. Oshiro, Lawrence S. Partika, and A1C Arthur T.Y. Lee. The classic anecdote of the evening was made by MSgt. Kang: "1955 (Summer Camp) was a bad year for us crapshooters, the "ole man" really cracked down on us-so, the younger men took up bowling and us older men took up beer drinking for recreation."

First Sergeant Theodore D. Hussey called the Nov. 4, 1946 roll and ten charter HANGmen answered "here" and front-and-centered to receive leis from the first lady of the squadron, Mrs. James R. Ashford: 1st Lts. (now Maj. S.) George R. Duncan, Walter F. Judd, Russell L. Smith, 2nd Lts. (now Capt. S.) Letwell P.A. Duvau-chelle, George T. Harris, SSgt. (now CWO) Joseph L. Barnett, SSgt. Harold J. Martinez, Sgts. (now MSgts.) Robert D. Choi, Richard S.O. Lee, and Sgt. (now TSgt.) Frederick H. Rittmeister.

An Open House and Air Show was held by the FIS Apr. 28, 1957 and drew an estimated 50,000 spectators. (The Project Officer was Lt. Col. John "Da Hawk" Giraud, AFSA.) Among other flying, 18 SABRE JETs fired up simultaneously, taxied out "in formation," and flew several tight aerial reviews. Static displays were provided by the HANG and other military services., the CAP drill team participated, etc. A picked maintenance crew changed a SABRE JET hot engine in record time (14 min. and 14 sec.): MSgts. Norman C. Ault, Anthony Kim, Clyde H. Sullivan, TSgts. George Joaquin, Lewis K. Kono, SSgt. Gordon L. Martin, A1Cs Samuel M. Saffery, Harry M.Y. Awana, and A2C Clement H. Crowell.

The outstanding event of the June summer camp (AC&W at home stations, remainder of HANG again at Kaneohe MCAS) was an aerial gunnery competition with the Marines-F-86E SABRE JETs vs. F-J4 FURYs. Four pilots of each five man team flew three missions with the standard 200 rounds chargeable when airborne. No alibis. The F-86Es fired at 20,000 feet (the Marines said the cal. .50 holes in the targets were "teensy weensy"), the FJ-4s at 18,000 feet (HANG pilots said the 20 mm holes in the targets were "bloody big").

HANG 1986



WHO'S AHEAD? The aerial gunnery competition between the HANG and the Marine Corps aroused great interest.

The HANG won, with a team average of 34.6% hits. The Marines were but 4.5% behind them. The winning HANG team was "dunked" immediately following the competition: Lt. Col. James R. Ashford, Capt. Lorenz L. Kumeilike, Frank J. Smith, Lts. Paul L. Hummel and Kurt E. Johnson. Capt. Kumeilike was the high man with an average of 50% hits. (HANGmen made out like bandits on the side bets!)

The HANG acquired a new air defense "boss" on June 22, 1957-the 326th Air Division (7th Air Force was deactivated.)

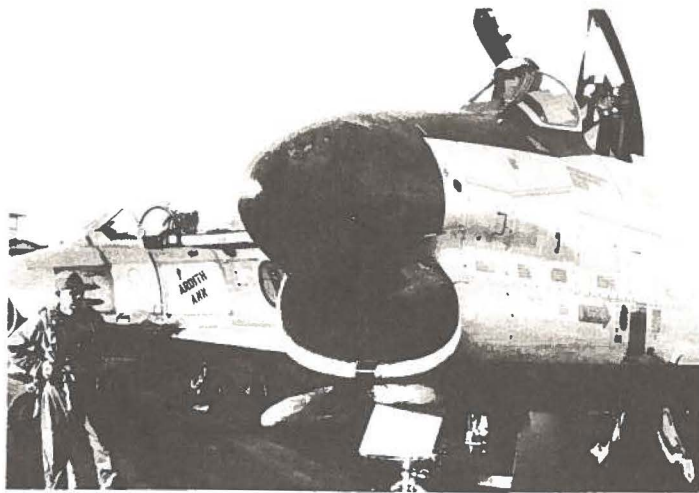
Sunrise to sunset active air defense with the day-fighter F-86E SABRE JET was fine, but what about air defense protection at night? The 109th AC&W Sq scanned the skies 24 hours a day at Punamano AFS, but what about the significant areas of approach to the island of Oahu that were "blacked out" by the mountains? USAF programmed the fighter squadron to have the F-86L all weather fighter interceptor SABRE JET and the 169th AC&W Sq. was to commence 24 hour active air defense at Koko Crater AFS. There was some lead time for implementation planning, and the HANG made the most of it.

Conversion from the day-fighter E to the all weather fighter L presented some good-sized headaches; the new engine had an afterburner, and space was inadequate in the Engine Shop; the L had a drag chute, and the HANG needed a larger parachute building; Supply had to secure some 7,000 new line items and plan to dispose of some 5,000 E line items; the L \$405,000 Flight Simulator was too big for the present Link Trainer building (Hickam moved out of a C-97 Flight Simulator and provided the HANG space in their Flight Simulator building), etc. The L had a much more elaborate fire control system (lots of "Little black boxes"! The armament was 2.75 inch "Mighty Mouse" air-to-air folding fin rockets instead of cal. .50 machine guns with which the HANG was very familiar. Changes in manning documents from day to all-weather fighter didn't occur until the middle of the next fiscal year. This caused numerous mal-assignments.

The pilots had to learn not only about the intricate systems of the new aircraft but also new tactics-"lead collision" instead of the familiar "pursuit curve." Head in the Bucket, son! Oh yes, there could be no stand-down of E active air defense during the conversion period.

The 169th AC&W Sq. was currently busy installing a new Search Radar (FPS-8) and allied equipment at Koko Crater AFS. The Sand Island facility was abandoned and the station of the 169th AC&W Sq. became solely Koko Crater AFS Dec. 15, 1957. The 169th commenced 24 hour active air defense Feb. 1, 1958.

During this fiscal year there was an infusion of new pilots: 1st Lt. John S. Carroll, 2nd Lts. Thomas A. Haine, Lloyd T.



DAY VERSUS ALL WEATHER. F-86E, Maj. George R. Duncan, and F-86L.

Hiraoka, Charles F. Johnston, George E. Kuroda, Marlin L. Little, Carl T. Nakamura, C. Hutton Smith, Melvin E. Souza and the man who would someday command the Hawaii Air National Guard—Edward V. Richardson.

The name of the game in FY 1959 was train, train, train.

All tactical pilots were checked out in the L by July 6, 1958. All were qualified to fly the L using visual pursuit curve tactics, but none were qualified Alert Ready or Combat Ready in the all-weather lead collision tactics. It was a whole new art of interception using complex fire control equipment: head in the bucket, son-you too can fly a radar scope! There were many hours of ground school, simulator rides, and three phrases of flying training missions for qualifications. Capt. Frank J. Smith and Lt. Carl S. Nakamura graduated with honors from the all-weather Interceptor School, Perrin AFB, Texas, and brought back much-needed information and techniques.

The inactive duty training schedule was changed in 1958 from two Sunday's a month to one weekend a month.

On Jan. 30, 1959, 2nd Lt. David W. Cowan and 1st Lt. Tai S. Hong were piloting a T-33 on a two-target mission. When at 30,000 north of the island of Kauai, their engine began vibrating and finally "froze". They could not release the target. They glided and ejected at 5,000 feet, some 20 miles north of Lihue. The two pilots were picked up from their seat dinghies an hour and 48 minutes later by a coast guard patrol boat.

A group of Thailand officers visited the new Alert Pad Mar. 6, 1959. King Hussein of Jordan did likewise two weeks later, and flew in a HANG T-33.

Annual field training was held at home stations June 13-27, 1959. PACAF conducted an Annual General Inspection and an Operational Readiness Inspection. There were 220 L sorties and over 600 rockets were fired the last day. Aircraft Maintenance averaged an 86.1% L in-commission rate versus 75% criteria. Turnarounds (loading rockets, fuel, servicing and inspections) averaged 11.3 minutes versus criteria of 15 minutes. The 199th and 109th were rated Fully Operationally Ready. In comparison with like-squadrons of PACAF, the 199th moved from last place to second place in combat ready status.

The end of the fiscal year found 32 of the 35 fighter pilots Combat Ready. Jet flying hours totalled 6,581, of which 5,052 were in the L. Average tactical pilot flying time was 188 hours.

Monthly meetings of Hickam Air Technicians began in an effort to weld the diverse functional areas into one team striving for common capability. The ORDER OF THE HAWAIIAN OWL was originated. A stuffed owl (genuine Pueo) was secured and mounted on an outsized base. The monthly winner was selected "who most distinguished himself

in any manner and for any cause deemed outstanding." Members were "entitled to stare fixedly and unblinkingly at each other or anyone else during duty hours or at other times they deem appropriate." Early winners included MSgt. Louis W. Perry, TSgts. George G. Wela, Ignacio Gabriel, Frederick H. Rittmeister and Mrs. Alice Doyle.

HANGmen celebrated Hawaii achieving Statehood Aug. 21, 1959. Pilots were standing by at Honolulu Airport when the official proclamations were delivered. Four F-86Ls were on the ramp waiting. And when the official announcements were delivered HANG pilots scrambled to deliver the documents to the Mayors of the neighbor islands.

The 199th Support Squadron was activated Feb. 1, 1960 with the nucleus of personnel from the inactivated 199 FIS augmentation. Lt. Col. Walter F. Judd was appointed Commander.

Col. Valentine A. Siefertmann, Assistant Adjutant General, Air/Commander HANG, was promoted to Brigadier General February 29, 1960—a leap year.

Lt. Col. James Ashford was transferred to HQ HANG Apr. 1, 1960, filling the new position of Deputy Commander, HANG. (He was promoted to Colonel May 20, 1960). Maj. George R. Duncan assumed command of the 199 FIS.

The HANG GOONEY BIRD supported recovery operations at Hilo following the seismic wave May 23, 1960, which killed 57 persons and caused extensive property damage.

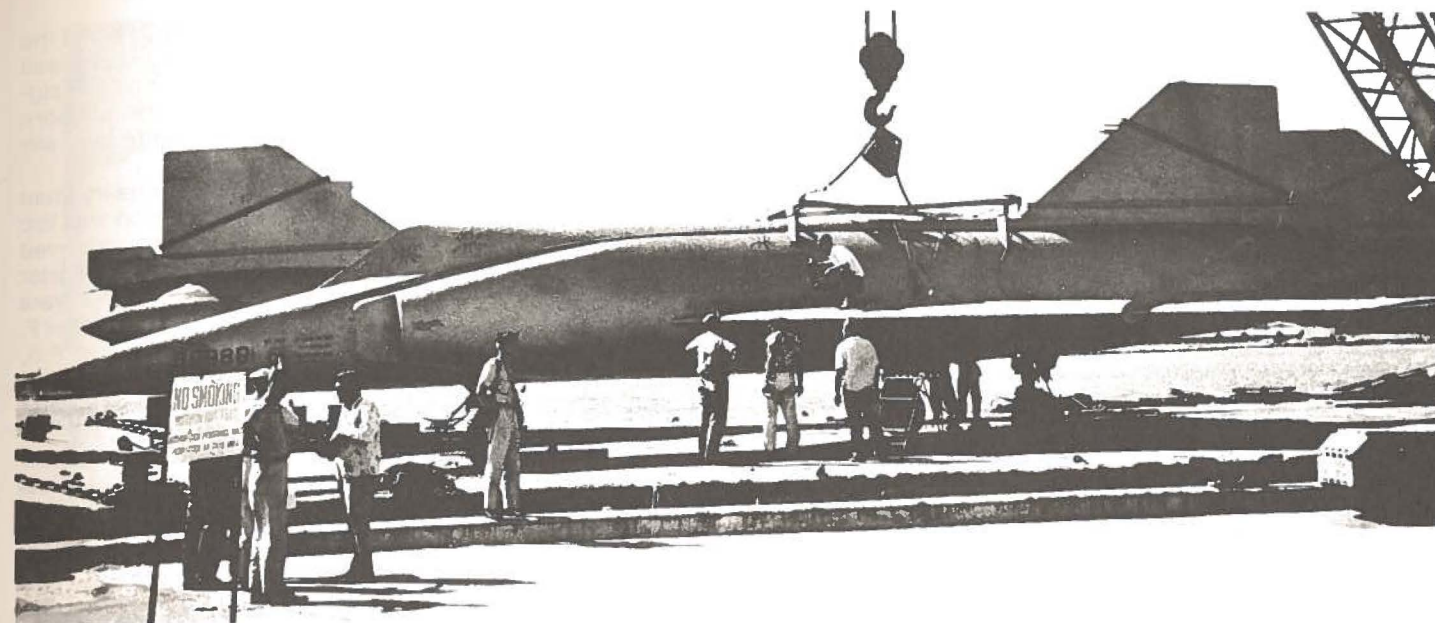
Thirty two Master Sergeant positions in the HANG were converted to the new "super grades" June 1, 1960; 22 became Senior Master Sergeant (E-8) and 10 became Chief Master Sergeant (E-9) positions. This made some "old heads" very happy . . .

Construction was started on a new radar facility at the old Kokee AFS, Kauai in June 1960. Completion was estimated at six months. The HANG also wanted to erect a radar site atop Haleakala, Maui, but the ensuing uproar by other users of that area killed the project.

Statistics for the year were impressive: 5,401 jet hours were flown (180 hours average per pilot); the 109th conducted 3,568 intercepts and the 169th conducted 3,087; there were eight emergency assists and 169 navigational assists; drill attendance of the 899 HANGmen averaged 95% and the reenlistment rate was 89%. The HANG Comptroller began IBM



ORDER OF THE HAWAIIAN OWL. MSgt. Louis W. Perry "entitled to stare fixedly and unblinkingly . . ."



COCOONED ARRIVALS. Art Harris supervising offloading of cocooned DEUCE from barge to dock.

punched card reporting to NGB. The HANG balance sheet showed \$26 million of federal assets used to support HANG operational efforts and \$6.8 million of Air National Guard funds were expended.

And all concerned themselves with planning for the forthcoming DEUCE.

The National Guard Bureau announced officially July 19, 1960 that the HANG was to be converted to F-102A Convair DELTA DAGGERS. HANG draft plans were rapidly finalized.

Two C-124s arrived July 20, 1960, loaded with the many components of an \$800,000 F-102 Flight Simulator. SSgts. Benjamin M.K. Goo, Guy M. Matsunaga, A1C Richard K. Kudo, and two Tech Reps began unpacking, assembly and checkout. It was located initially in a C-124 nose dock at Hickam AFB. Two months later the big day came—final checkout. When power was applied there were a few puffs of smoke. Problems were minor, and the simulator was operational and in full scheduled use several days later.

Six fire control specialists were sent to the 40th FIS, Yokota, Japan for a cram OJT course on the new MG-10 Fire Control System: MSgt. David M. Rodrigues, TSgts. Gary K. Fujii, Allen M. Mizumoto, SSgts. Thomas S. Araki and Virgil Brown. Forty Aircraft mechanics were sent to the ANG unit at Kelly AFB, Texas to learn about the DEUCE airframe, engine, instruments, electrical systems, etc. Four pilots also went to Kelly AFB to check out in the DEUCE. (Around-to-clock 5 minute active air defense Alert was still being maintained in the Ls back at Hickam.)

The DEUCE was armed not only with folding fin rockets but with Falcon air-to-air guided missiles. A test console was set up in an old Hickam munitions storage structure by SSgts. Lloyd T. Seki, Gordon Y. Makishima, A3C Daniel J. Placido, and Hughes Tech Rep. The missiles were checked out as they arrived.

Base Supply was extremely busy as it was procuring some 12,500 DEUCE parts as well as maintaining sufficiency for the Ls. Line items increased during this year from 8,600 to 19,500. The workload was high; 54,957 documents were processed versus 25,293 the previous year.

HANG Hickam units were expanded from two squadrons to a six unit air defense fighter group Dec. 1, 1960. Lt. Col. Robert B. Maguire was appointed Commander of the new 154th Fighter Group. The 199th was reorganized and the 199th Support Squadron inactivated. Resultant Squadron Commanders were: Maj. Kenneth L. Fisher, 199th FIS; Maj. Tyler M. Harr, 154th Air Base Sq.; Maj. Jarrett F. Carr, 154th CAMRON; Maj. William W. Dau, 154 Material Sq.; and Maj. Claude V. Caver, 154th USAF Dispensary.

The first DEUCE arrived in Hawaii Dec. 5, 1960. Land Air Inc. handled de-cocooning and processing, and delivered ready birds to the HANG commencing in January 1961.

HANGmen's brothers-in-arms, the 298th Artillery Group (Air Defense), Hawaii Army National Guard, began 24 hours active air defense Alert with their NIKE surface-to-air missiles Dec. 15, 1960.

Meanwhile the pace of the 109th HANGmen at Punamano AFS began to accelerate rapidly. They formed a Detachment under the command of Maj. Robert W. Liddle to operate the rapidly completing the \$323,310 operations facility at Kokee AFS, Kauai.

Capt. Donald M. O'Day ramrodded the complicated transfer of personnel from Oahu to Kauai—for there had to be leapfrog continuity of ACW active air defense from Punamano AFS to Kokee AFS.

At 8:30 a.m., Mar. 15, 1961, "B" crew began active air defense operations from Kokee AFS. "A" crew left for Kauai that afternoon, and the leapfrog was completed.

The F-102s replaced the F-86Ls on active air defense Alert in May 1961. The "honest Ls" were phased out and sent to Hickam AFB for salvage.

Maj. Arthur U. Ishimoto was awarded the Hawaii National Guard Medal for Merit; he designed and projected the ACW facility and equipment at Kokee AFS. Capt. Raymond J. Moriguchi (Det. 1, 109th) was awarded the George T. Miki Memorial Trophy for outstanding and noteworthy performance of Weapons Director duties, 1st Lt. Edward V. Richardson received the Outstanding Interceptor Pilot Award, and MSgt. David M. Rodrigues was awarded the Outstanding HANG Airman Award.

The FAA approached HANG and proposed a joint effort to establish a radar station at Mt. Kaala. The proposal was: (1) the FAA to build the access road, (2) HANG to provide the radar equipment and (3) FAA/HANG to provide equal sums of money to develop the station.

Capt. William E. Mossman was detached from the 169th and took over command of Det. 1, 109th AC&W Sq. at Kokee AFS, Kauai, Jul. 1, 1961. Capt. Donald M. O'Day, Ops Officer of Det. 1, returned to Oahu and became Ops. Officer of the 169th to Koko Crater AFS.

This was the year that the word "professionalism" began to creep into HANG jargon. Concepts and procedures became formalized and were closely adhered to. The "Texas Plan" of Year Round Field Training, rather than 15 consecutive days of summer camp, came into being. The units scheduled their men for 10 separate days of training throughout the year and



annual field training, was reduced to five days. Emphasis shifted to a better quality of On-the-Job (OJT) Training, and personnel were scheduled duty to assist during times of peak workloads. The one weekend a month "drill" was modified so that there was emphasis on flying on Saturdays; Sundays were devoted to OJT and ground school. There was emphasis on Disaster Control training; operations under fall-out conditions were realistically exercised (Battery Selfridge became very hot after a few hours of "taking shelter" there).

The DEUCES were modified to Configuration Seven, which gave increased radar capability against aircraft employing electronic countermeasures. Construction was started on a new Missile Facility and an Aerospace Ground Equipment Shop at the new fighter facility.

Capt. James A. Forsyth of the 199th Weather Flight had his civilian job transferred to Hilo, Hawaii in September 1961. He began commuting to Oahu to drills, most of the time flying a small plane himself. This certainly established a HANG record of initiative to maintain perfect attendance.

The 150th AC&W Sq. was activated Oct. 21, 1961, with station at Kokee AFS, Kauai. Capt. William E. Mossman was appointed Commander. The 109th AC&W Sq. at Punamano AFS had a resultant reduction in its personnel authorizations. (The HANG redoubled its efforts to have the 109th mission changed to mobile communications.) Capt. Mossman made sure the island of Kauai knew that the 150th was there—each of the 92 Air Technicians received one month's pay in 100 silver dollars and the remainder in paper currency. Kauai banks and merchants were impressed.

The 154th Ftr. Gp. new hangar complex was completed Nov. 6, 1961, and the access taxiway to Honolulu International Airport completed 11 days later. The group began the move from Hickam's Area 65, which took several months.

Saturday, Feb. 17, 1962, was the "Grand Opening." The 10 AM Dedication Ceremony speakers were: Governor William F. Quinn, Maj. Gen. F.W. Makinney (Adj. Gen.), Brig. Gen. John A. Rouse (Cmdr. Hawaii Air Defense Division) and Brig. Gen. Valentine A. Siefermann (Cmdr. HANG). HANGmen were in rigid formation.

The huge hangar doors slowly opened after the speeches.



Governor Quinn cut a maile lei across the entrance and the Roosevelt High School Choir inside the hangar sang Hawaii Pono. A high speed (and low) pass by a single DEUCE signaled the end of the official ceremony—the scramble horn sounded, and two pilots "broke ranks" and ran to their aircraft and were airborne within five minutes.

The momentous day ended with a 15th Anniversary Luau (actually three months overdue, but the occasion was too much to miss). Over 900 HANGmen and guests were served at tables set up in the new hangar. The following "charter members" of the HANG were still with the outfit and were present: 1st Lts. (now Lt. Cols.) George R. Duncan, Walter F. Judd, Russell L. Smith, 2nd Lt. (now Maj.) Letwell P.A. Duvauchelle, SSgt. (now CWO) Joseph L. Barnett, Sgts. Robert D.W. Choi (now SMSgt.) and Frederick H. Rittmeister (now MSgt.)

Authorizations of the 199th FIS were changed Apr. 1, 1962 to provide for more field grade officers. After all, these pilots were flying a bird which cost over a million and a half dollars.

Maj. Peter R.M. Fowler assumed command of the 154th Air Base Squadron, Apr. 16, 1962.



THE EYES OF HANG ARE UPON YOU. Clockwise from lower left corner: George R. Duncan, Walter F. Judd, Russell Smith, Fred Rittmeister, Robert "Bobby" Choi.

The HANG settled into training to maintain a high level of operational capability. A System Exercise was conducted one weekend assembly a quarter, usually with a predawn call. HANGmen became accustomed to the idea that they were on two hours notice, day or night. The tactical pilots averaged 135 flying hours this year.

1st Lt. Vernon Ah Nin lost his canopy behind Tripler Army Medical Center while on an active air defense Alert flight in Aug. 1962. Loss of the canopy caused quite a draft, but Lt. Ah Nin pulled down the visor on his helmet and made it back okay.

Prince Carlos of Spain visited the 154th Ftr. Gp. Aug. 8, 1962.

The 154th Ftr. Gp. competed with six other Air Guard F-102 outfits in the September 1962 Ricks Trophy Event at Tyndall AFB, Fla. The HANG 27-man team, captained by Lt. Col. George R. Duncan, traveled to and from "America" in an Oklahoma ANG C-97. They first went to Webb AFB, Texas and borrowed some DEUCES from the USAF 331st FIS. The team "peaked" the birds and held a pre-competition to determine the two pilots to represent the HANG. Capt. Harold Nagai and 1st Lt. Ritchie K. Kunichika were selected. Scoring during the Ricks meet was based on firing live missiles at a drone and an evaluation of the entire team. The HANG team placed third, with Lt. Kunichika found the "third hottest interceptor in the Nation".

Col. James R. Ashford assumed command of the 154th Ftr. Gp. Oct. 1, 1962; Lt. Col. Robert B. Maguire became HANG



NEW HOME. The new fighter facility at Fort Kamehameha.

Director of Operations. Other command changes during the fiscal year included: Lt. Col. George R. Duncan assumed command of the 154 CAMRON (Jul. 1, 1962), Maj. Dewitt C. Bye assumed command of the 109th AC&W Sq. (Jul. 31, 1962), Maj. Jarrett F. Carr assumed command of the 199th FIS (Jan. 1, 1963), and Capt. James A. Forsyth assumed command of the 199th Wea. Flt. (Jun. 1, 1963).

PACAF presented its Tactical Unit Flying Safety Award to the 154th Ftr. Gp. in October 1962 for an accident-free safety record. The 154th USAF Dispensary vigorously supported OPERATION SWALLOW, a community effort to control polio this same month.

Lt. Reginald Y. Doi of the 150th AC&W Sq. conducted an impressive emergency assist Oct. 12, 1962. A Navy twin engine attack bomber (A3D) enroute to Hawaii from Alaska, with seven aboard, declared an emergency (lost) with 40 minutes of fuel remaining. Lt. Doi discovered the bird 140 miles out and vectored the A3D to Bonham Naval Auxiliary Landing Field, Kauai. When the aircraft safely landed, there was fuel for but another few minutes flying time.

President John F. Kennedy made some comments about the physical fitness of Americans. At 4:00 a.m., Mar. 2, 1963, 11 "Blister Seekers" of the 150th AC&W Sq. began a 50-mile walkathon from Mana to Anahola. Only one of these "Mad Strollers" completed, and it took him 16:10 hours to do it: SSgt. Wayne Y. Soma.



HANG'S FIRST CHIEF SUPER SERGEANT. Col. James R. Ashford, 154th Ftr. Gp. Cmdr., presents chevrons to new SMSgt. Robert D.W. Choi.

SMSgt. Robert D.W. Choi became the first Chief Master Sergeant of the HANG Apr. 21, 1963.

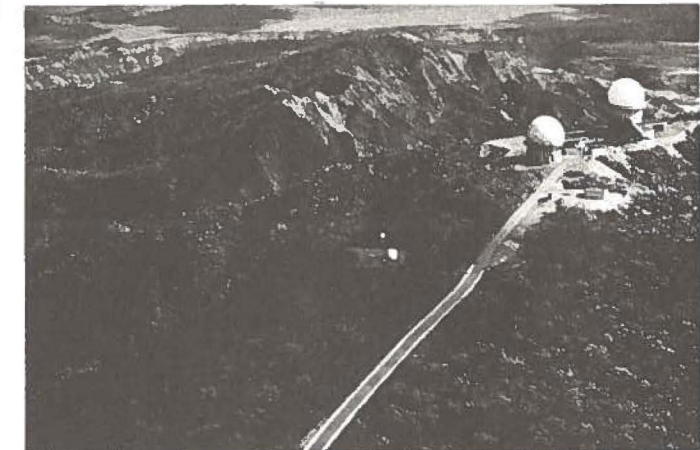
OPERATION BIG SWITCH occurred during the five-day June summer camp (there was no Operational Readiness Inspection this year.) Fifty HANGmen cross-trained at the Air Guard's 130th AC&W Sq. in Utah; a like number of UTAHmen cross-trained at the 109th, 150th, and 169th AC&W Sqs. This worthy effort should really have been called Operational Goya, for it was the "brain child" of Lt. Col. Kiyoshi P. Goya, Commander of the 169th.

FY 1963 ended on a sad note—1st Lt. Charles F. Johnston was killed Jun. 21, 1963 when his DEUCE plunged into the ocean 34 miles southeast of Oahu.

The HANG Consolidated Base Personnel Office (CBPO) was established July 1, 1963 under the supervision of Maj. James K. Lota.

The 154th Air Base Sq. and the 154th CAMRON were deactivated July 20, 1963 (the aircraft maintenance function became part of the 154th Material Sq.) The 154th Combat Support Sq. was activated the same day, which replaced the air base sq.

Two detachments were authorized under the group headquarters. Command changes during the fiscal year included:



MOUNT KAALA RADAR STATION.

Maj. Tyler M. Harr to command of the 154th Material Sq. (July 20, 1963), Maj. Dewitt C. Bye to command of the 150th AC&W Sq. (Aug. 7, 1963), and Maj. Frank W. White to command of the 154th Combat Support Sq. (Sept. 15, 1963).

Progress on the Mt. Kaala Complex was in full swing. Since the facility was to be joint-use with the FAA and HANG, a joint-use agreement was negotiated and finalized. The FAA was responsible for maintenance and certification of the search radar, however, the maintenance section was jointly staffed by FAA and HANG personnel.

Three DEUCES flew a "missing man formation" in final tribute to the late John F. Kennedy. The pilots were: Col. James R. Ashford, Maj. Donald M. Giddings and Jon G. Parrish.

King Kalakaua's birthday, Nov. 16, saw the fulfillment of another gigantic "scrounge job" (the usual number of many things "discovered" by the HANG)—for the first time in 70 years the streets of Honolulu town saw the Royal Guard of the Kingdom of Hawaii. The men wearing colorful blue and white uniforms and white pith helmets, and carrying vintage 45-70 Springfield rifles with fixed bayonets, were volunteer HANGmen tangibly expressing pride in Hawaii's military heritage.

Modern blue-suited HANGmen were at rigid attention during the ceremony when the re-created Royal Guard emerged from their old home, Iolani Barracks. Governor John A. Burns stated in part during his address: "The

traditions of the past are means by which people gain dedication, by which they gain strength to meet the trials of the present and the future."

Requirement for membership in this elite group of HANGmen was that each must have at least one drop of Hawaiian blood. All commands were in Hawaiian, the drill was of the Monarchy period. Organization and authenticity were the result of the efforts of Lt. Col. Walter F. Judd. Leaders of the Royal Guard were elected by their own members: SMSgt. Theodore D. Hussey became Kapena Moku (Capt.), MSgt. Adam Y. Aki became Luna Koa (Lieut.), and Maj. James K. Lota the He Kakiana (Sgt.).

The following year started off on a sad note. Maj. James Y. Sato was killed the evening of Jan. 7, 1964 when his DEUCE crashed into the sea while approaching Honolulu International Airport for landing.

Col. Frances Gabreski, the top living USAF fighter Ace (40 kills during World War II and Korea), was attached to the 199th FIS for flying while he was assigned to Hq PACAF.

Non-Commissioned Officers (NCOs) of the 150th AC&W Sq. organized the Hui-O-Heka-Kiana (Club of Sgts.) Mar. 15, 1964. Msgt. Charles H. Tanaka was the first President.

Air Force Reserve Col. Arthur Godfrey, the noted Radio and TV personality, visited the 154th Ftr. Gp. July 14, 1964 and was briefed on HANG's active air defense. He also had a flight in a TF-102 piloted by Maj. Harold Nagai.

NCOs of the 169th AC&W Sq. organized an NCO Club on July 19, 1964. MSgt. Kenneth S. Kiyabu was elected the first President.

Maj. Donald O'Day assumed command of the 109th AC&W Sq. Aug. 1, 1964.

General Hunter Harris, PACAF Commander, checked out in the DEUCE Sept. 22, 1964. His final check ride in the TF-102 was with Maj. Kurt E. Johnson. He commented about the HANG: "The finest in the nation—a unit of which Hawaii can be proud."

The 169th introduced a new innovation into its drill schedule in October—split drills. This gave more opportunity for concentrated on-the-job training of all personnel, as the number on duty at any one time was small.

Twenty-four members of the Royal Guard celebrated their first anniversary Nov. 16, 1964 at Iolani Palace where they were inspected by Gov. John A. Burns. It is an event they have celebrated annually since.

A number of "firsts" occurred during January 1965: The 154th Ftr. Gp. won NGB's OJT Training Unit Achievement Award. Navy intercept Controllers began proficiency training with the 169th AC&W Sq. at Koko Crater AFS. HANG's Royal Guard was used by the Governor as part of the official greeting of Japan's Prime Minister Eisaku Sato when he arrived Jan. 15. The Royal Guard had an even greater honor on Jan. 20 when they were selected to represent Hawaii at President Lyndon B. Johnson's Inaugural Parade in Washington, D.C. (The long march down Pennsylvania Avenue in 25 degree weather was made as comfortable as possible with concealed "long johns".)

Twenty new DEUCEs, which had formerly been stationed at Okinawa and the Philippines, were received in March. They were modified to Configuration Eight, which gave them an Infra Red sight capability and they could carry different missiles.

The 150th AC&W Sq. began to use Kekaha Armory, "at the bottom of the hill", for their administrative headquarters.

Maj. Harold T. Mattos assumed command of the 154th Material Squadron May 11, 1965.

The 169th had a huge handful of things to do: the Mount Kaala Complex was completed and the \$3 million gear was checked out, there was an ORI, 20 of their men were in Utah, and a leap frog move to their new home was imminent.

The 169th closed up shop at Koko Crater AFS, July 1, 1965 and at the same instant "opened shop" at the multi-stationed Mount Kaala Complex: operations and radio maintenance at Building 204, Wheeler AFB; radar maintenance jointly with



CARRY THE STATE COLORS WITH HONOR. Governor John A. Burns presents the Hawaiian flag to Kapena Moku Theodore D. Hussey of the re-created Royal Guard.

the FAA atop Mt. Kaala; and radio-relay to/from the 150th on Kauai at Punamano AFS.

The 109th AC&W Sq. ended its activities at Punamano AFS and moved to Koko Crater AFS July 1, 1965. Maj. Paul S. Mori assumed command of this proud senior AC&W Sq. August 1st.

The 154th Consolidated Aircraft Maintenance Squadron and the 154th Supply Squadron were activated Dec. 1, 1965 and the 154th Material Sq. was inactivated. Maj. Harold T. Mattos and Frank Carlos, respectively were appointed commanders of the new squadrons.

The 6.8 mile road to Mt. Kaala was closed for three weeks in January 1966 because of a landslide. The 169th radar maintenance men had to walk over a break in the road for three weeks with the assistance of a safety line—it was "like walking on marbles on a steep hill".

Maj. Daniel K.C. Au assumed command of the 154th Supply Sq. May 1, 1966.

Maj. Kurt E. Johnson assumed command of the 199th FIS on July 1, 1966.

Brig. Gen. Valentine A. Siefermann was appointed Acting Adjutant General July 15, 1966 when Maj. Gen. Robert L. Stevenson resigned.

There was a "Name the C-54" contest. (A C-54 SKYMASTER had replaced the old VC-47 GOONEY BIRD the year before.) TSgt. Wayne Y. Soma won with his suggestion, "Spirit of Aloha", and received a \$25.00 savings bond. (Any of you old heads remember the "Spirit of St. Louis"?)

The HANG became 20 years old in November 1966. There was a grand Open House at the Hickam fighter facility on Oct. 20. A DEUCE appeared on display at Kapiolani and King Streets the morning of Oct. 28. Air Guardsmen in blues and their Ladies filled the Hilton Hawaiian Village Dome on the 5th of November for a gala birthday party. A deluge of rain filled the streets to sidewalk level and made entry interesting. The following "charter members" of the HANG were still with the HANG and present: 1st Lt. (now Col.) Walter F. Judd, George R. Duncan (now Lt. Col.), Russell L. Smith (now Lt. Col.), Sgts. Robert D.W. Choi (now CMSgt) and Frederick H. Rittmeister (now MSgt.).

There was another landslide on the Kaala road in November which took a week to clear and rebuilt. Capt. Edward V. Richardson was awarded the HNG Commendation Medal Mar. 1, 1967 for planning and carrying out safeguards for the 154th Ftr. Gp.'s complex weapons system from November 1965 to June 1966.

The 201st Mobile Communications Squadron was activated Mar. 6, 1967 with initial duty station at Battery Selfridge. Maj. Paul S. Mori was appointed Commander. The 109th AC&W Sq. was inactivated. By the end of the fiscal year the 201st was at 35% strength and 40% of its new equipment



HOME OF THE 154th FIGHTER GROUP.

had been received. (The four flights were added Oct. 13, 1967.)

The 154th Fighter Group received a National Guard Bureau Award for an accident-free 1966.

A short time later, on Mar. 21, Capt. George E.M. Joy bailed out at 500 feet when his DEUCE flamed out while on approach to Runway 8, Honolulu International Airport. He landed through high-tension wires at Iroquois Point. His bird went into the drink at the entrance to Pearl Harbor.

A week later, on Mar. 30, two DEUCEs piloted by Maj. John G. Parrish and 1st Lt. James S. Haraguchi took off from Dillingham AFB. Somehow they brushed wing tips, and went on to Hickam for minor repairs.

The 150th achieved a record 13,620 radiate hours on their high value magnetron tube in April 1967—the longest lived magnetron in PACAF. The "Maggie" was affectionately known as "Caroline".

Governor John A. Burns represented the United States at the coronation of His Majesty King Taufa'Ahau Tupou IV of the Tonga Islands on July 4, 1967. The HANG C-54 "Spirit of Aloha" was asked to transport the official party from American Samoa to Tonga and return.

Maj. Herbert S. Isonaga assumed command of the 154 Combat Support Squadron on Sept. 1, 1967.

The 150th AC&W Sq. mourned the loss of "Caroline", their famed magnetron tube, which finally failed after serving for

17,122 radiate hours. In comparison, Air Defense Command had a standard of 5,000 radiate hours.

Maj. Tai S. Hong assumed command of the 199 FIS on Jan. 20, 1968. Two days later the 199th FIS received its first AFCAT (Alert Force Capability Test). One hundred hours of ECM (Electronic Counter Measure) flights were made available in February.

A3C Timothy L. Albao provided another example of dedication—he achieved two years of commuting from Hilo, Hawaii to Kauai for weekend assemblies with his outfit, the 150th AC&W Squadron.

The new 201st MCS was growing and expanding at a rapid rate. The four flights were authorized Oct. 13, 1967 and ninety more positions were authorized in March 1968. NGB was generous with in service school allocations, and the 201st enlisted 185 non-prior service airmen during the fiscal year. PACCOMMAREA conducted an inspection April 5-7 which found, understandably at this early date, that the unit was not operationally ready.

Team training soon began and equipment was twice deployed to Dillingham AFB and set up. The MRN-13 Tower was deployed to Kona Airport in May while the normal tower there was under construction. By the end of the fiscal year the 201st had received 70% of its equipment. Authorization was also received for a 201st facility.

PALACE ALERT was an Air National Guard program to augment USAF air defense units in Southeast Asia (suggested by the HANG) and Europe. Combat ready DEUCE pilots from 20 Air Guard units were eligible to volunteer. Maj. Jon G. Parrish and Gerald K. Sada were the first two HANG pilots to volunteer for 90 day tours. They went to the Philippines and flew active air defense missions from Da Nang, South Vietnam. They were followed by Lt. Col. Kurt E. Johnson, Maj. Ritchie K. Kunichika and Edward V. Richardson, and Capt. Rexford H. Hitchcock and Marlin L. Little.

Maj. Melvin G. Yen was flying a DEUCE 115 nautical miles from Honolulu July 13, 1969 when his AC power failure warning light came on and oil pressure began to fluctuate. He turned towards Honolulu and climbed—making it to 34,000 feet. The low oil pressure light came on, engine vibration started, RPM dropped to 90%, and his adjacent Flight Leader noticed white smoke coming from the aft end of Maj. Yen's DEUCE. Engine temperature climbed through 800 degrees, there was a minor explosion and the engine froze (POI 62 was still 54 miles from Honolulu). Maj. Yen was able to "glide" to a safe landing at Barbers Point NAS. Maj. Yen was awarded the



PALACE ALERT VETERANS. Maj. Ritchie K. Kunichika, Edward V. Richardson, Gerald K. Sada, Capt. Rexford H. Hitchcock and Marlin L. Little.

Air Force Association's Earl T. Rick's Trophy and received PACAF's Able Aeronaut Award for saving his aircraft.

The 201st MCS was not about to be outdone in comparison with the HANG air defense missioned units in active mission support. They deployed radio gear to Hilo several times in support of Military Airlift Command (MAC) participation in several LEPRECHAUN LAUGHTER operations, and sent teams to Pago Pago, Samoa to support the comm requirements for APOLLO 8, 9 and 10. It sent 28 men to Thailand to support the requirements of SEATO Exercise SEA SPIRIT jointly with USAF's 1st Mobile Communications Group. The 201st also made nine other local training deployments.

Capt. Thomas A. Haine of the 199 FIS was Captain of the United States Volleyball Team that competed in the October 1968 Olympic games in Mexico City.

Four DEUCEs flew a diamond formation over Hickam AFB mall at sunset Nov. 2, 1968. This signaled the start of joint military religious services honoring those who gave their lives in Southeast Asia.

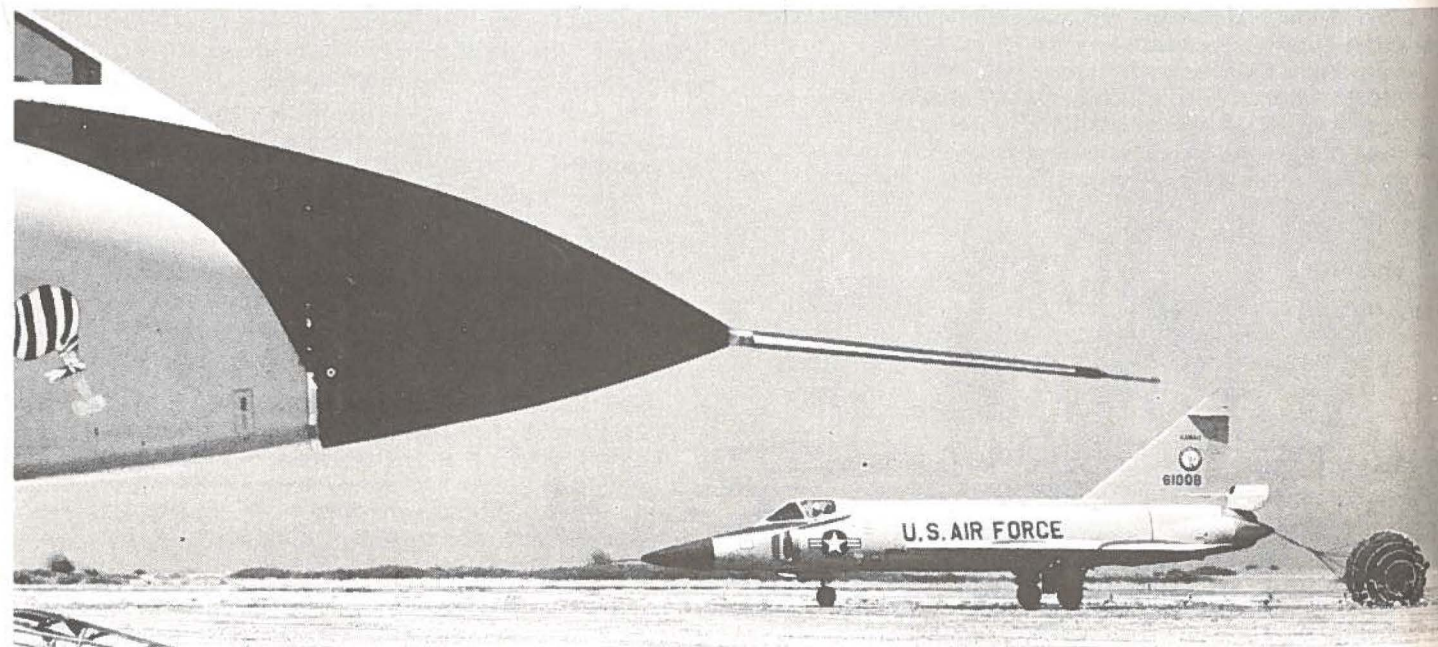
Air and Army Technicians changed status from State employees paid with Federal funds to Federal employees Jan. 1, 1969. Among other things, this generated much paperwork and formalized personnel procedures.

Quite a storm hit the island of Kauai on Jan. 5. PMRF Barking Sands recorded 80 knot gusts of wind; damage to the facility was extensive, including the HANG hutments. The 150th recorded 34.03 inches of rain in 24 hours at Kokee AFS!

Did you see the movie about the Japanese attack on Pearl Harbor, Dec. 7, 1941—TORA TORA TORA? Did you know that four HANG pilots were "actors" when the film was shot January-April 1969? The following piloted "Japanese" attack aircraft: Cols. James R. Ashford and Robert B. MacGuire (ZEROS), Maj. Kenneth J. Costa (VAL, KATE & ZERO) and William T. Klopp (KATE & ZERO).

Supply was F-102 NORS-free (Not Operational Ready, Supply) until April 13, 1969 with a record 1,266 days (nearly 3 and 1/2 years) behind them. Finally aircraft maintenance needed something and supply couldn't deliver. Was it something like a wing or an engine? No, it was a butterfly valve. Great credit goes to the tremendous efforts to achieve this record, and well within the 154th Supply Squadron motto: "One team, one mission—keep 'em flying".

PACCOMMAREA inspected the 201st MCS and found that it was "progressing favorably towards becoming an effective mobile unit". There were a tremendous number of 3 level, a shortage of 5 level, and a very critical shortage of 7 and 9 level



DEUCE!

AFSCs.

HANG units had a splendid parade June 15, 1969, the first all-HANG parade in many years. Governor Burns was Reviewing Officer and made the following presentations: two HNG Commendation Medals, Outstanding Interceptor Pilot Award, George T. Miki Trophy, and Outstanding HANG Airman of the Year Award. Maj. Gen. Benjamin J. Webster presented five Air Medals to DEUCE pilots who had participated in PALACE ALERT. Brig. Gen. Harold Johnson, Commander of the Pacific Communications Area, presented a plaque to the 201st MCS and 28 certificates of merit to the 201st members for their excellence in SEA SPIRIT.

Supply received PACAF's "Outstanding Achievement Award" for its NORS-free record. Commendations were also received from San Antonio Air Material Area and the National Guard Bureau.

There were 1,493 HANG personnel assigned at the end of the fiscal year—84.3% of unit manning documents and 95.1% of NGB programmed strength. Drill Attendance averaged 97%.

Fiscal Year 1970 was a tough year by any standards—service schools were cut, money was reduced for supplies, and there were three aircraft accidents.

On Sept. 12, 1969, Maj. Thomas Phelan of the 326th Air Division was on his take-off roll when his right landing gear strut failed due to material failure. The DEUCE veered to the right and rode on the right wingtip until the aircraft stopped. The right external fuel tank was dragged off and caught fire, which was quickly extinguished by the alert and excellent firecrash crews.

Two months later, Nov. 15, 1969, 1st Lt. James D. Moncrief was approaching Honolulu International Airport's Runway 8 for a landing. His engine flamed out, and two attempts to restart were unsuccessful. He steered the DEUCE off shore and ejected at 1,000-1,500 feet. Lt. Moncrief landed some 200 yards off shore among four surfers, borrowed a surfboard, and paddled to shore. His survival training two weeks previously in Okinawa really paid off.

Then on May 27, 1970, Capt. Melvin E. Souza was on his takeoff roll when the fire warning light came on and mobile control told him that his aft section was on fire. Throttle back, drag chute out, brakes applied, fuel shut off, electrical power off, canopy open—when the DEUCE stopped, Captain Souza was out and running as an efficient fire department swung into action.

Tough times, but no one was injured.

News was received in September that the 298th Artillery Group (NIKE) of the Hawaii Army National Guard would no longer have the mission of ground-to-air active air defense and would be inactivated. The HANG was authorized to go 100% Air Technician manning and 24 NIKEMen became HANGmen.

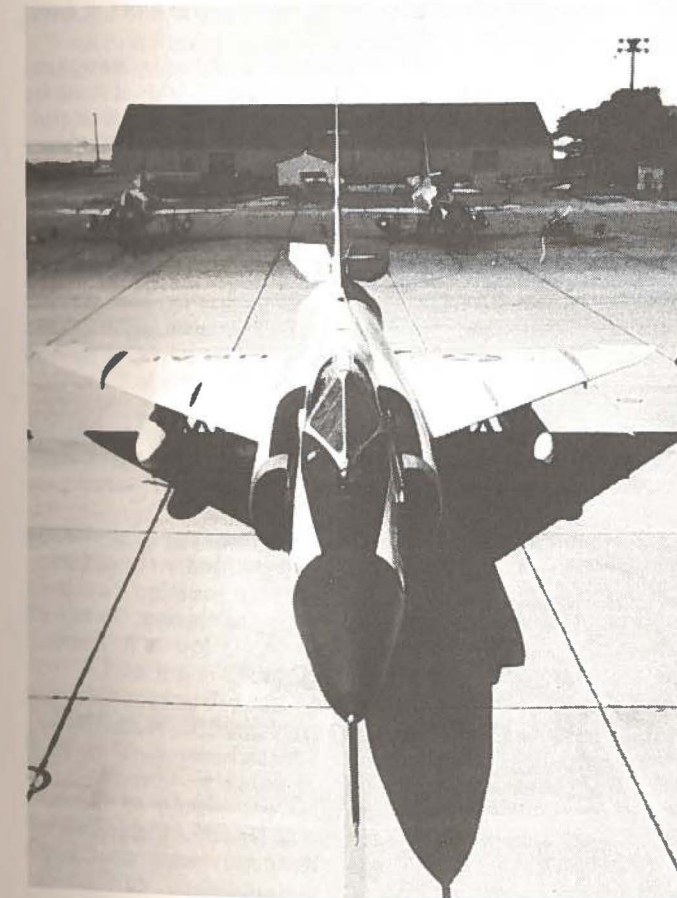
1st Lt. Hiram H.S. Pang assumed command of the newly activated the 154th Civil Engineering Flight, Oct. 18, 1969.

Throughout the years since Nov. 4, 1946 the 199th Weather Flight steadily maintained their excellence and mission accomplishment capability. It was named the Outstanding Air National Guard Weather Flight for 1969.

The bouncy new 201st Mobile Comm Squadron received the Air Force Outstanding Air National Guard Mobile Communications Squadron trophy on May 10, 1970. This trophy was won for outstanding mission potential in 1969 in competition with 43 like units. The 201st also received PACCOMMAREA Safety Award for 1969. Fiscal Year 1970 was a busy year for the 201st; it supported the Military Airlift Command with air-to-ground radio and technicians on five separate occasions at General Lyman Field, Hilo, Hawaii; Apollo 12 with two telecommunication operators at Pago Pago, Samoa; and support of the 154th Ftr. Group at three locations during spring camp.

Maj. Casimer Jasinski assumed command of the 154th USAF Dispensary, June 1, 1970; Lt. Col. Claude V. Caver was reassigned to Hq HANG as Air Surgeon.

The fiscal year ended with 1,604 HANG personnel assigned, 100.1% of programmed strength. Of these, 478 were full time employees (Air Technicians). The pilots flew 5,300 DEUCE hours, Aircraft Maintenance supported 75.2% F/TF-102 aircraft in-commission rate. Supply supported with an average 0.5% monthly NORS rate. The AC&W Squadrons conducted over 14,700 intercepts. The \$42 million of USAF



DELTA DAGGER!

HANG 1986

assets were well used. Federal funds expended in support of the HANG was \$12.9 million; State of Hawaii obligated funds totaled \$125,459.

Maj. Lawrence C. Cabrinha and Capt. George E.M. Joy participated in PALACE ALERT—flying air defense missions in Southeast Asia. They were the last two, as the program was terminated.

NGB allocations were further reduced in FY 1971. As the Vietnam War wound down so did Defense spending. HANG money managers sharpened their pencils and great efforts were made to assure that all users got the word that FY 1971 was going to be a REAL austere year.

PACAF conducted a bang-up Annual General Inspection July 18-28, 1970. The summary included the favorable comments that the HANG was managing its assigned mission well and that the F-102 Simulator Section was considered the best in PACAF.

Secretary of Defense Melvin R. Laird issued a significant policy Memorandum Aug. 21, 1970: "A total force concept will be applied in all aspects of planning, programming, manning, equipping and employing Guard and Reserve Forces . . . attention will be given to the fact that Guard and Reserve Forces can perform peacetime missions as a by-product or adjunct of training with significant manpower and monetary savings . . . Guard . . . units will be prepared to be the initial and primary source for augmentation of the active forces in any future emergency requiring a rapid and substantial expansion of the active forces."

Interestingly enough, this important policy statement was in accord with the Hawaii Air Guard's evolution throughout the years. The possibility of updating HANG's mission equipment stirred considerable interest among HANGmen . . . It also reversed the prior National policy of manning for the Vietnam conflict. It reaffirmed that the Air Guard would indeed be the Ready Reserve of the United States Air Force.

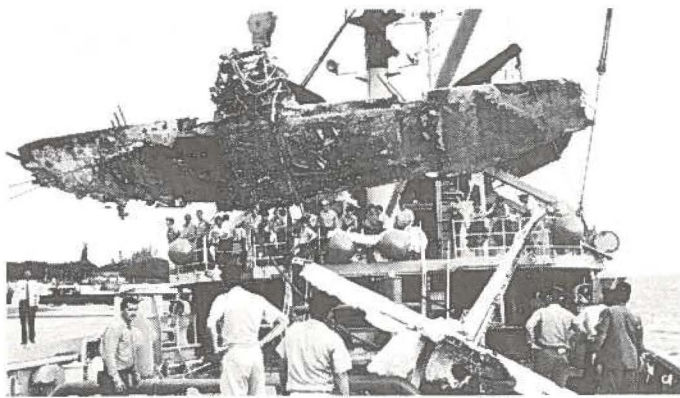
The new \$1 million facility at Fort Kamehameha for the 201st MCS was completed and dedicated Sept. 1, 1970. 201st personnel were happy to move into their shining new facility—with all due respect for their prior temporary home at Battery Selfridge.

The 201st was busy as usual with deployments. The unit supported MAC's PROJECT OPPORTUNE JOURNEY at Hilo, APOLLO 14 at Pago Pago, American Samoa, and emergency maintenance support to PACCOMMAREA for a Radio Beacon problem at Okino, Okinawa. The 201st received three AN/GRM-94 Maintenance Shelters, an AN/TSW-7 Air Traffic Control Central and three AN/MRC-108 Communications Centrals to add to their impressive long list of mission equipment on hand.

The 169th AC&W Sq. was awarded the Air National Guard Distinguished Communications Electronics Maintenance Unit plaque at the 92nd NGAUS Conference, New York City, in September 1970. This proud HANG unit placed second in competition with all ANG C-E units.

Air defense communications improved Dec. 3, 1970 when the inter-island tactical microwave system between Kokee AFS, Kauai and Wheeler AFB, Oahu became operational. The Radio Relay Station at Punamano was deactivated.

The 154th Ftr. Gp. had acquired an F-86E and an F-86L for heritage display. Capt. Edward H. Anderson had been given the project of finding an F-47 (JUG) to complete the display of all tactical aircraft which the HANG had flown. Diligent publicity and almost a ream of correspondence over several years had proven unsuccessful. A JUG in flying condition was discovered in "America" that might be available for \$18,000. That idea was discarded (no money), and it was estimated that it would take five years of Portuguese sweetbread sales during drills to accumulate that huge sum.



F-47 THUNDERBOLT, or at least part of one, found in waters off Waimanalo. The guns still worked!

It was in 1971 that Capt. Eddie Anderson found a World War II F-47 Thunderbolt in 20 feet of murky water off Waimanalo. Unfortunately, during recovery operations by the U.S. Navy, the heavy fighter was broken into three pieces. One amazing fact: the .50 caliber machine guns were removed from the wings, cleaned by the weapons shop and successfully fired after spending 27 years at the bottom of the ocean!

February 1971 marked the 10th year of operations with the F-102. And when we got 'em, they were already ten years old!

Several modifications were added to the DEUCE to extend the life of the air frame and improve engine reliability.

By November 1971, the HANG Headquarters staff consisted of these key people: Commander-Brig. Gen. Valentine A. Siefertmann; Executive Officer-Col. Walter F. Judd; Director of Operations-Col. Robert A. Maquire; Air Operations Officers-Lt. Cols. Harold Nagai and William T. Klopp; Staff Officer-Lt. Col. William E. Mossman; Director of Material-Lt. Col. Arthur U. Ishimoto; Medical Officer-Col. Claude V. Caver; Director of Personnel Management-Maj. Richard M. Settsu; Comptroller-Lt. Col. Richard S. Furuno; Civil Engineering Staff Officer-Capt. Cedric D.O. Chong.

MSgt. William Tabe of Operations at the 150th AC&W Sq. saw the year this way, "I'll always remember 1971 because that was the last year that we went to summer camp as a unit. It was one of those old time 'summer camps' that was so enjoyable."

Maj. Owen Ota won the George T. Miki Award for his outstanding performance as a weapons controller with the 150th.

In the area of community activity, we registered 8 on the Richter scale in 1972. We worked on the Cancer Crusade, gave hundreds of young people tours of the Fighter Group and the Royal Guard made 16 public appearances. This was the year that Mayor Frank F. Fasi appointed Maj. Eddie Anderson chairman of the city-wide "Great Automobile Round-up" and the 154th participated with all other military services in rounding up 2,500 derelict and junk cars which were dumped into the ocean to provide breeding ground for reef fish. Our motor pool "whiz" B.K. Meyer, "Hancho" for the HANG team, moved more cars than any service in cleaning up the toughest area: Windward Oahu.

In support of the State civil disturbance mission, the 150th AC&W Sq. organized, equipped and trained a 50 member civil disturbance task force under the command of Maj. John English to handle any civil disturbance on Kauai.

Lt. Col. Paul Goya was awarded the Air Force Commendation Medal in August for his role in administering the PACAF Weapons Controller Requalification School. And the 169th AC&W Sq. was presented the NGAUS Air National Guard Communication-Electronics Trophy, having competed with 102 organizations nationally.

The 201st MCS (now the Combat Communications Group) participated in 12 deployments in support of the Air Force, including deployment of telephone maintenance technicians

to Osan AFB, Korea; VHF radios and technicians to General Lyman Field in Hilo, Hawaii (on seven separate occasions); VHF transceivers to the Marine Corps Air Station, Iwakuni, Japan; VHF transceivers to Kaneohe MCAS; and one mobile control tower to Kwajalein Island.

Joining the HANG staff in 1972 were Maj. John Carroll as Legal Staff Officer and Major Kenneth D.O. Chong as Management Analysis Officer.

Enlisted technicians at the headquarters were Sergeant Major-CMSgt. Clifford Chee, SMSgt. Cornwall Matsusaka, SMSgt. Kenichi Sonan, and MSgt. Clifford Wong.

The strength of the HANG was 1569, a loss of 35 personnel from the previous year.

The total strength of the Guard was to become a major concern in Hawaii and across the nation as the end of the draft became imminent. The draft had been a prime motivator of young men in their effort to join the Guard as an alternative to active duty service. The advent of the volunteer force would eliminate the draft motivated of Guard enlistees.

The first WAFs were enlisted in HANG in 1972—Lt. Barbara O'Hare and A1C Phyllis Dickens. Iris Kabazawa was the first NPS female enlistee.

The airmen retention rate remained relatively high for the fiscal year with 72.6% either reenlisting or extending their enlistments.

Attendance at Unit Training Assemblies (UTAs) in HANG was a high 97%. This resulted in a well-trained and prepared force, as was proven in several exercises conducted by PACAF's 326 Air Division.

The 201st Combat Communications Group (CMBTCG) assisted in nine Opportune Journey deployments. Of particular importance were the VHF radios provided to the 1st Mobile Comm Group during "Operation Homecoming," the return of the POWs in January 1973.

The retirement in 1973 of Maj. Gen. Benjamin Webster, after six years of service as Adjutant General, resulted in the promotion of Brig. Gen. Siefertmann to Major General and his appointment to the position of Adjutant General. Col. Robert A. Maguire was promoted to Brigadier General and became the new HANG Commander.

The Air Force's Total Force Policy meant a major change in operational style for the 199th Weather Flight; it meant moving into base weather stations to work directly with the active duty forces during drills. The unit's mobile meteorological van was scrapped because of poor maintainability and the improbability of its use in an actual emergency.

Capt. Mike H. Tice received the HANG Outstanding Interceptor Pilot Award and MSgt. Delbert D. Dandurant, Combat Support Squadron, earned HANG Outstanding Airman of the Year.

Our alert status remained the same: Two aircraft on "five-minute" and two on "30 minute." We racked up 5,014 flying hours in the F-102, providing air time for 42 pilots to maintain their combat readiness.

The 154th held its first annual Family and Friends Day in 1973 when more than 1,200 people visited shops and witnessed a scramble, airborne intercepts, and weapons loading.

Lt. Col. Jon G. Parrish and 1st Lt. Fred Bradley terminated their assignments with the 199th and three new second lieutenants joined the pilots' ranks after graduating from the Air Force undergraduate pilot training program and the ANG F-102 Combat Crew Training School at Houston, Texas. They were Robert F. Darrow, Robert H. St. Clair III and Peter S. Pawling.

The 154th Civil Engineering Flight assisted the Air Force by deploying to Osan AFB, Korea, to erect a new chapel for the base during their 15-days of annual training.

It was late in 1973 that all personnel were required to wear those little blue plastic name tags. This was also the year that the Social Actions Office was created, headed by Capt. Meriem "Sascha" Saromines. SSgt. Janet Cox, meanwhile became the first woman to be accepted into the 150th AC&W

Squadron.

"I don't know if the squadron was ready for us, but I know I was ready to see if I could compete in a 'man's world' of ACW operations," SSgt. Cox said. She's in a man's world now—HARNG.

The 169th AC&W Sq., meanwhile, underwent a reorganization. The unit's authorized strength was reduced from 235 to 180. The ground power and communications center sections were deactivated and the air police section was reactivated. However, there were no changes in the unit's mission or location.

The recruiting of WAFs continued with considerable success into 1974. Most units were now partially staffed with women volunteers. This resulted in an immediate improvement in morale and a general upgrading of the language in some of the formerly all-male shops. The number of women in the Guard increased from 8 to 25, with 2 officers and 23 enlisted.

The C-54 continued to perform admirably, although the aircraft was beginning to show signs of wear. The decreasing number of C-54s in active service throughout the United States pointed to the fact that parts and supplies for this aircraft might also become very scarce.

Lt. Col. Herbert Isonaga, long-time commander of the 154th Combat Support Squadron, retired and received the HNG Commendation Medal. His aloha tea house party will be long remembered. We told Herb later that he had a great time.

The HANG also said "Aloha" to retiring Lt. Col. Frank Carlos at a luau at the Japanese Chamber of Commerce hall in February. This was also a sad year with the death of MSgt. Frederick "Ritt" Rittmeister. Ritt's dedication, perseverance and "can-do" attitude exemplified the foundation on which the HANG had been built.

On Feb. 28, Col. Walter F. Judd, HANG Director of Personnel Management, retired. Col. Judd was known as the "Father of the Royal Guard". Maj. Donald O'Day, operations officer of the 169th AC&W Sq., was selected to succeed Col. Judd as DPM.

"The thing that I remember about 1974 was the aggressive recruiting drive conducted by the 150th AC&W Sq. I should know, I was one of the 32 who enlisted," recalled Alvin Honda. Recruiting played a big part in the HANG in 1974 with the elimination of the draft.

On July 1, 1974, a white Hawaii Air National Guard recruiting van (self-mobile, 34'6" long, 8' wide and 10'8" high) made its first appearance in public at the State Farm Fair held at the Honolulu International Center.

The conversion of the inside of this former x-ray van was done by the 154th Civil Engineering Flight. Many people had a hand in putting this van together and manning it at different events, thus showing the continued dedication of HANG personnel.

The 169th AC&W Sq., historically an all-male outfit, was liberated May 17, 1974 with the enlistment of AB Aurora Lee. And Amn. Donna E. Culnan became the first WAF to sign up with the 199th Weather Flight.

In August 1974, Lt. Col. Edward V. Richardson assumed command of the 199th FIS.

The 150th AC&W Sq. was tasked with supporting RIMPAC 74, a multi-national naval exercise involving Australia, New Zealand, Canada and the United States. The Commander, Communications-Electronics Warfare Systems Group commended the 150th Control Team for the outstanding support provided during the exercise.

Fiscal year 1975 again saw movement of personnel within the headquarters. Col. Arthur U. Ishimoto became Acting Chief of Staff and Lt. Col. Richie K. Kunichika became Acting Director of Operations. Lt. Col. John S.W. Lee was transferred from the 199 FIS to become Director of Safety, Lt. Col. Hong returned to the 154th Fighter Interceptor Group.

Personnel strength began to show signs of rebounding from the slump caused by elimination of the draft. The re-

creased manpower and in increased enthusiasm within the units. A total of 61 additional slots were filled during the year as HANG moved to 90.7% of its authorized manning level.

In February 1975, Task Force Guard went into action. The civil disturbance control element of the Hawaii Air National Guard deployed to Oahu Prison with the Army Guard to conduct searches for contraband and weapons. Lt. Col. William E. Mossman of HQ HANG directed the operation. After the initial phase, the HANG men augmented the Army Guardsmen who remained at the prison until new prison security guards could be trained.

February also saw the retirement of the C-54 "Spirit of Aloha", the victim of old age and corroded wing spars. Over a thousand C-54s had flown since the first one left the assembly line in 1942. The "Spirit of Aloha" was one of the last in the active inventory of the Air Force/Air National Guard.

The 199th Weather Flight spent four days of annual training at Kilauea Military Camp on the Big Island and apparently took the "weather with them". It rained the entire time they were there. One day, the Hawaii Volcano Observatory's rain gauge recorded 19 inches of rainfall in 24 hours. At that time, Hilo had experienced approximately 22 consecutive days of rain. Pretty good timing for the Weather Flight—a "non-prophet" organization.

Unlimited post and base exchange privileges on any two days of the month for HANG members was a big step forward in 1975.

In March, the 199th FIS and the 150th AC&W Sq. gave full support to RIMPAC. Once again forces from the United States, Canada, Australia, and New Zealand joined together and for two weeks mock air, surface, and sub-surface battles took place in Hawaiian waters. The "enemy" air forces were offered the full facilities of the 199th for briefing, debriefing and data collecting. The 199th flew many "attack" sorties against naval forces at sea.

The HANG participated in Memorial Day services for the late Governor John A. Burns at the State Capitol and arrival ceremonies for the Queen of England at the Honolulu International Airport.

Lt. Col. Daniel K.C. Au, Commander for nine years of the 154 Supply Squadron, was promoted to Deputy Commander for Logistics. Assuming command of the squadron was Maj. Gerald T. Morihara.

More than 3,000 individuals from many schools, clubs and organizations toured the 154th Fighter Interceptor Group during this year. Additionally, 1,600 members, families and employers participated in the third annual open house in mid-June.

The 169th received its initial exposure to the Tactical Air Control System by performing the role of Control and Reporting Center (CRC) in its active support of Exercise Cope Elite. This was a Joint Chiefs of Staff coordinated, Commander-in-Chief Pacific sponsored joint exercise conducted in support of the U.S. Army's Exercise Opportune Journey held from April 16 to June 24.

Headquarters USAF and the National Guard Bureau approved a proposal to reorganize the existing 201st MCS and



PHANTOM and DEUCE

four separate flights into a mobile communications group with the addition of a mobile communications group headquarters of approximately 43 military positions.

Lt. Col. Kiyoshi P. Goya was assigned as the new Commander of the 201st Mobile Communications Group. Colonel Goya commanded the 169th AC&W Sq. before assuming command from Lt. Col. Paul S. Mori.

In addition, approval was received to plan the relocation of four of the five units located at Hickam AFB, Oahu, to three of our neighbor islands.

The selected locations included: The new Group headquarters (43 positions) and the 202 MCF (123 positions) to remain at the facility at Hickam; the 202 ATCF (59 positions) relocated to PMRF Barking Sands, Kauai with plans for a \$200,000 facility; the 201 MCF (123 positions) was projected to be relocated to Kahului, Maui into a new \$800,000 facility; the 201 ATCF (59 positions) was projected to be relocated to Keahole Airport to a new \$200,000 facility; and the 201 MCS relocated to Keaukaha Military Reservation (HARNG) adjacent to General Lyman Field until a new facility could be constructed. The group headquarters, mobile communications squadron and flights were redesignated as "combat communications" units.

Many personnel changes again took place at HQ HANG during FY 1976, beginning with the retirement of Brig. Gen. Robert A. Maguire. Col. Arthur U. Ishimoto was named to succeed General Maguire and in moving to the HANG Commander position, became the first non-rated officer to hold it. The Chief of Staff position vacated by Col. Ishimoto was filled by Lt. Col. John S.W. Lee. Lt. Col. Donald M. O'Day was transferred from the headquarters to assume the position of Commander of the 169th AC&W Sq. Col. Claude V. Caver retired as Aerospace Medicine Physician and was succeeded by the former 154th USAF Clinic commander, Lt. Col. Casimer Jasinski.

Information officer, Maj. Edward H. Anderson, returned to the 154th FIG. Maj. Allen M. Mizumoto became the Acting Director of Logistics and was promoted to Lieutenant Colonel.

It was early in 1976 that PACAF nominated the 199th FIS for consideration as recipient for the coveted Hughes Trophy for its performance in 1975. Although the 199th was not selected for the Trophy, it was an honor to be chosen by PACAF over all of its regular Air Force units in the Pacific area, the largest Air Force command in the world!

The conversion of the 154th FIG to F-4C PHANTOM aircraft began to occupy considerable time at HQ HANG in planning and coordination. More than just a change of aircraft, the F-4C required an additional crew in the form of weapons systems officers, additional maintenance training and facilities, pilot training, additional supply coordination, and a myriad of other details.

Adding to the burden of pilot training was the acquisition of the C-7 Caribou to replace the retired C-54. Pilots were required to train on this new aircraft as well. HQ HANG provided two of the pilots for the C-7: Lt. Col. John S.W. Lee and Lt. Col. Ritchie K. Kunichika. The new crews were trained in time to support the new requirements of the reorganization of the 201st Combat Communications Group (CMBTCG). The C-7 began making runs to Hilo and Kauai to support and equip the units newly assigned to the neighbor islands.

The year 1976 saw the culmination of several years of patient effort by HANG members Lt. Col. John Carroll, SMSgt. Kenneth Kiyabu, and others in passing a bill defining State responsibility for the use of guard members called to State active duty. The landmark bill was welcome news to any member facing a call-up.

During the May UTA, the 169th AC&W Sq. was presented the Air Force Outstanding Unit Award for exceptional and meritorious service in support of military operations from July 1973 to June 1975. Maj. Gene. Charles C. Patillo, Vice Commander-in-Chief of Pacific Air Forces, made the presentation.

Recruiting and retention remained focal points in FY 1976, as manpower levels hovered near the 90% mark. The move by units of the 201st CMBTCG to the neighbor islands focused much of the recruiting emphasis to Kauai and Hawaii.

In late 1976, the F-102s were retired and the F-4s were put on runway alert. One of the DEUCEs was retained for our Aircraft Hall of Fame to be displayed with our other "old warriors."

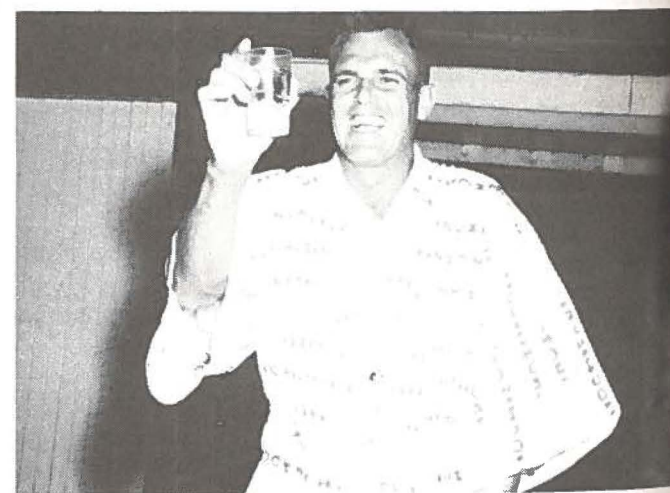
Changing missions and changing personnel have characterized the headquarters of the Hawaii Air National Guard from the beginning. Serving as a focal point for the many requirements of its subordinate units, the headquarters allocates, evaluates, adjusts, and assigns its assets of people and material to meet the assigned missions. With new capabilities provided by the switch to F-4C PHANTOMS, new problems posed by the moves to the neighbor islands, and continued requirements for close scrutiny of recruiting and retention, the mission of the headquarters is likely to remain as it has been—coping with change.

Thus brings to a close the first 30 years of the Hawaii Air National Guard. But the HANG has continued to grow. As you will see in coming pages the HANG has come a long way in the last ten years, from 1976 to 1986, and promises even more progress in the future with the coming of the F-15 EAGLE.

Our deepest gratitude to our predecessors, without whom, the Hawaii Air National Guard, as we know it today, would not have been possible. And our thanks to those who have dedicated so much time to the documentation of the history of Guard members, their spirit of perseverance, and the roots from which we've grown.

This 30 year history was condensed from "HANG 25" and "HANG 25 PLUS 5" on which we decided we could not improve. And so we extend a great big "Mahalo" to Col. Walter F. Judd (HANG 25) and Maj. Edward H. Anderson and Capt. James B. Wiley (HANG 25 PLUS 5), and to the many others who contributed much time and effort in the researching of these two books, without whom this historical account would not have been possible. Special thanks also to CMSgt. Theodore H. Hussey and Colonel Judd, who provided technical assistance in the production of this condensed account.

We truly established in this thirty year period, and continued in the next ten years to maintain and carry with us into new areas, HANG's TRADITION OF EXCELLENCE.



HAPPY 40th ANNIVERSARY, HANG! That appears to be the toast offered by Lt. Col. John G. Giraudo, Air Force Senior Advisor from June 19, 1956 to Jan. 19, 1959. Colonel Giraudo made a significant contribution to HANG's proficiency in the air.

HANG COMMANDERS PAST...

Headquarters, Hawaii Air National Guard (HQ HANG) was organized on July 1, 1952.



Maj. Gen. Valentine A. Siefertmann
(Commanding Officer of HANG, 1953-1973)



Brig. Gen. Robert A. Maguire
Commanding Officer of HANG, 1973-1976



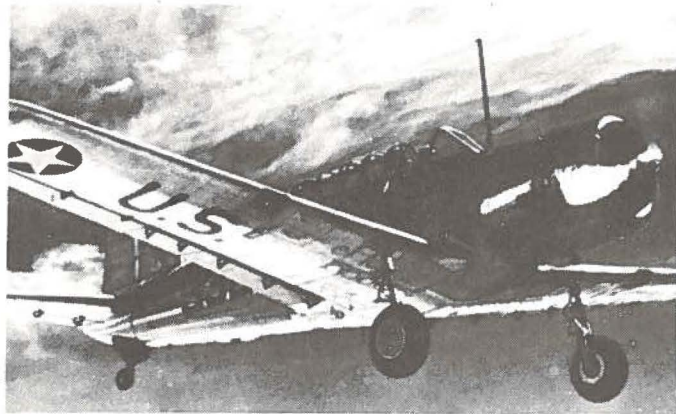
Maj. Gen. Arthur U. Ishimoto
Commanding Officer of HANG, 1976-1982

...AND PRESENT



Brig. Gen. Edward V. Richardson
Commanding Officer of HANG, 1982—Present

HANG AIRCRAFT HISTORY



AT-6. One of HANG's early birds.



P-47 (later designated F-47) THUNDERBOLT, 1947-1954. Our first fighters were affectionately known as JUGs.



B-26. One of the first aircraft assigned to the HANG along with the two aircraft types shown above. (1952)



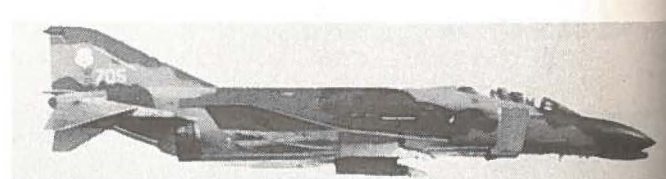
F-86E SABRE JET, 1954-1958. The HANG was no longer propelled by props. With this bird we entered the Jet Age.



F-86L ALL WEATHER INTERCEPTOR, 1958-1961. Took HANG from daytime operations to 24 hours a day.



F-102 DELTA DAGGER, 1961-1976. The DEUCE served the HANG for 15 years.

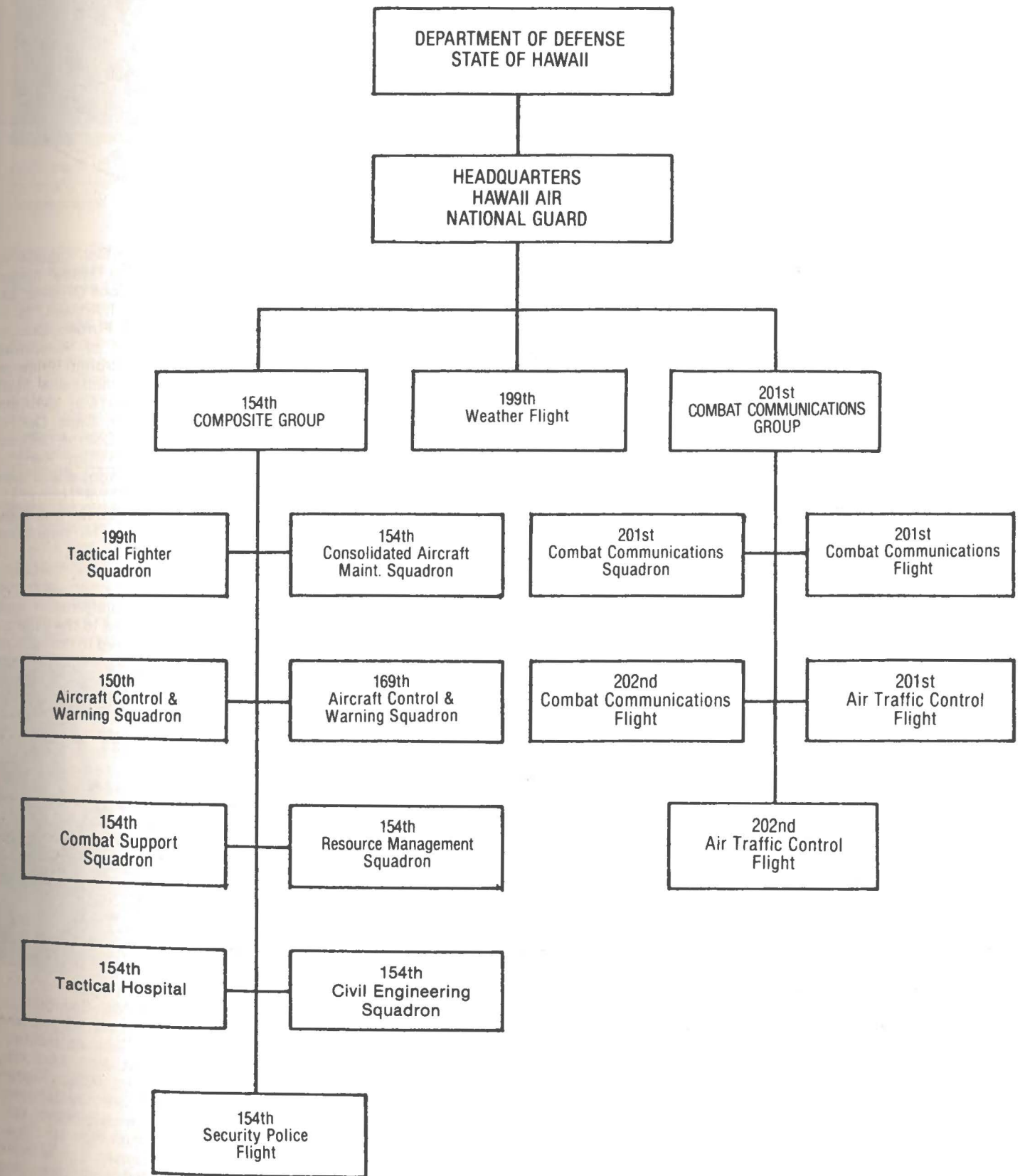


F-4C PHANTOM, 1976-1987. Extended range and air refueling capability took HANG Pacific-wide.



F-15 EAGLE, 1987-(?). Our next big step forward, the EAGLE will take HANG farther and faster into the future.

HAWAII AIR NATIONAL GUARD ORGANIZATIONAL CHART





Brig. Gen. Edward V. Richardson
HANG Commander

HQ HANG



official emblem

Although the Hawaii Air National Guard (HANG) was federally recognized on Nov. 4, 1946, it was not until Jan. 1, 1952 that the Headquarters, Hawaii Air National Guard was authorized.

The original HQ HANG staff included Lt. Col. Valentine Siefertmann, Chief of Staff; Lt. Col. Frank R. Harlocker, A-3; Maj. Vernon Flake, Air Inspector; WOJG Frank Carlos, Supply Officer; and six enlisted personnel.

Lt. Col. Siefertmann was promoted to Colonel in February 1955. He was appointed Assistant Adjutant General, Air/Commander HANG and promoted to Brigadier General on February 1960. A month later, Lt. Col. James R. Ashford was transferred to HQ HANG from the 199 Fighter Interceptor Squadron (FIS) to fill a new position of Deputy Commander, HANG. He was promoted to Colonel on May 1960.

Also in 1960, Maj. Arthur U. Ishimoto was heavily involved in designing and building the radar facility at Kokee AFS, Kauai. He made frequent trips to the island and established his headquarters at the quaint Hale Pumehana Hotel in Lihue. He was awarded the Hawaii National Guard Medal for Merit for his accomplishment.

The HANG Airmen Council was organized in May 1961 to advise Commander HANG on the concerns of the enlisted personnel. The members were MSgts. Harold K. Abe, Richard N. Fukui, Theodore D. Hussey, George H. Oshiro, John O. Sierra, Lino Fernandez and Henry J. Kajiwara.

In August 1964, MSgt. Clifford M.S. Chee replaced MSgt. Wing Kong Chang as the Sergeant Major of HQ HANG. Three months later a young and vivacious Sandra Maruyama (nee: Kanemoto) was hired as a secretary-stenographer. About the same time, Airman Basic Douglass Souza joined the HQ HANG staff. Little did he realize that he would set a record as the only member in the history to HANG to serve his entire 20-year military career at the state headquarters. For his efforts, he was awarded a well deserved Air Force Commendation Medal.

In 1968, the HNG Civilian personnel was organized with CMSgt. Kozen Kaneshiro as one of the original members and later followed by the assignment of MSgts. James S. Oshiro and Ralph M. Kaneshiro and Mrs. Kim Oe Yuen.

In September 1971, the Hawaii National Guard Association hosted the annual National Guard Association of the United States conference at the new Sheraton-Waikiki Hotel. With General Siefertmann as the general chairman for the conference, HQ HANG personnel became involved in the myriad of tasks required to host a major conference involving thousands of conferees and guests. The highlight of the conference was the All States Dinner attended by more than 2,000 guests in the hotel's ballroom.

By 1972, HQ HANG was fully manned by experienced personnel. The staff included General Siefertmann, Commander; Col. Walter F. Judd, Executive Officer; Col.

Robert B. Maguire, Director of Operations; Col. Claude V. Caver, Medical Officer; Lt. Col. Harold "Big Daddy" Nagai and William T. "Klippity" Klopp, Air Operations Officers; Lt. Col. William E. Mossman, Staff Officer; Lt. Col. Arthur U. Ishimoto, Director of Materiel; Lt. Col. Richard S. Furuno, Comptroller; Maj. Richard M. Settsu, Director of Personnel Management; Maj. Allen Mizumoto, Electronic Systems Officer; Lt. Col. Herbert Isonaga, Communications Staff Officer; Maj. John Carroll, Legal Staff Officer; Maj. Kenneth D.O. Chong, Management Analysis Officer; Capt. Cedric D.O. Chong, Civil Engineering Staff Officer; CMSgt. Clifford M.S. Chee; SMSgt. Cornwal Matsusaka; SMSgt. Kenichi Sonan and MSgt. Clifford Wong. The headquarters was located in Building 5, an old converted World War I barracks. The old timers who remember Building 5 swear that they shared the building with someone who was often heard but not seen.

With the retirement of Maj. Gen. Benjamin J. Webster, Adjutant General, in 1973, there were major changes to the HQ HANG command structure. General Siefertmann was appointed as Adjutant General and promoted to the rank of Major General. Colonel Maguire was promoted to the rank of



1981: Headquarters staff.

1st Row (left to right): SSgt. Joyce Akagi, Maj. Edwin Miyahira, SSgt. Vivian Toma, SSgt. Blossom Lee, MSgt. John Sabey, Brig. Gen. Arthur Ishimoto, Col. Casimer Jazinski, and CMSgt. Harry Awana; 2nd Row: Capt. Jerry Matsuda, MSgt. Ralph Kaneshiro, SSgt. Pearl Duy, SMSgt. Kozen Kaneshiro, MSgt. James Oshiro, and A1C Timothy Williams; 3rd Row: MSgt. Douglass Souza, Maj. Charles Prentiss, Maj. Wayne Yamasaki, and Col. Kazuo Yamamoto; 4th Row: CMSgt. Clifford Chee, Lt. Col. Robert Konishi, Col. Edward Richardson, Col. Kenji Sumida, Lt. Col. David Rodrigues, and TSgt. Randall Tom.



Building 5, Fort Ruger, served as the headquarters building for many years. It was a converted WW I barracks that old-timers swear was "haunted".

Brigadier General and became the new HANG Commander. Maj. Edward H. Anderson joined the HQ HANG staff as the Information Officer. Maj. David P. Wissmar became the new Civil Engineer Staff Officer. New titles were given to Lt. Col. Nagai, who became Director of Operations, and Lt. Col. Ishimoto, who became Director of Logistics.

In addition, new faces appeared at the headquarters scene. Lt. Col. Richard Tokairin, Lt. Col. Kenji Sumida, Lt. Col. Kazuo Yamamoto, Maj. Ian Komenaka, Maj. Richard Lee, MSgt. John Sabey, Sgt. Randall Tom, and Sgt. Katherine Castillo Asato became members of the staff. Sergeant Asato was the first female guardsperson assigned to the headquarters. CMSgt. Chee vividly recalls cooking T.V. dinners during Unit Training Assemblies (UTAs) on a two-burner hot plate. An old stove was procured from the Group to replace the hot plate, but the dinners were still burnt.

1974 saw new faces in HQ HANG with Lt. Col. Tai Sung Hong was the new Chief of Staff and Director of Operations, Lt. Col. Ritchie K. Kunichika as Air Operations Officer, and Lt. Col. Donald M. O'Day as Director of Personnel Management. Lt. Col. Nagai was reassigned as Commander of the 199 FIS while Lt. Col. Settsu left to join the 154 Fighter Interceptor Group (FIG).

In February 1975, the civil disturbance control element of HANG deployed to Oahu Prison under Task Force Guard to conduct searches for contraband and weapons. Lt. Col. Mossman of HQ HANG directed the operation.

Again, personnel changes took place at HQ HANG during 1976 with the retirement of General Maguire. Colonel Ishimoto was named to succeed General Maguire as Commander of HANG and was subsequently promoted to Brigadier General. Lt. Col. John S.W. Lee was appointed as Chief of Staff, Lt. Col. Casimer Jasinski was appointed Aerospace Medicine Physician upon Col. Caver's retirement, and Maj. Allen M. Mizumoto was appointed Acting Director of Logistics and promoted to Lieutenant Colonel.

Considerable amount of time was devoted by the HQ HANG staff to the 154th's conversion to the F-4C aircraft and establishment of the 201st Combat Communications Group. In addition, Lt. Col. Lee, Lt. Col. Kunichika, Lt. Col. Sumida, Lt. Col. Tokairin, Maj. Komenaka, and Capt. Tanaka from HQ HANG were tasked to fly the C-7 Caribou to support the units on the islands of Hawaii, Maui and Kauai.

After several years of patient efforts by HANG guardsmen, including Col. John Carroll, Col. Daniel Au, Lt. Col. Kenneth Chong, Lt. Col. Herbert Isonaga, SMSgt. Kenneth Kiyabu and others, the State Legislature passed bills defining the responsibility for the use of guardsmen called to state active duty; providing State tax credit for guardsmen and tuition waiver for guardsmen while attending the University of Hawaii; making the uniform code status of military justice apply during inactive duty.

HANG 1986

In 1977, General Ishimoto held the first annual HANG Commanders and Operations Conference at Kauai Surf Hotel in Kauai. Emphasis was placed on planning for the future of HANG. Key NGB and PACAF personnel were invited to participate in the conference. This conference has continued to grow in stature and has become one of the major events of the HANG's calendar.

Also in 1977, HQ HANG held its first live-in summer camp at Fort Ruger. The staff "roughed" it in the armory and in later years enjoyed the luxuries of the Hawaii Army Guard bandroom. Col. Lee was the only staff member who avoided the live-in.

Shortly thereafter, the HQ HANG underwent a major reorganization. Eight full-time technicians were transferred to the 154 Composite Group (COMPG) to implement the tri-deputy organization. With only six full-time staff members remaining, greater reliance was placed upon the utilization of drill status personnel assigned to HQ HANG. During this period of turmoil, drill status officers Lt. Col. Sumida, Lt. Col. Yamamoto, Lt. Col. David Rodrigues, Maj. Wayne Yamasaki and Maj. Jerry Matsuda were called upon to perform many of the full-time functions. Evenings spent away from their families were not unusual occurrences. Toiling late at night together, however, did bring a special cohesiveness to the group. Lt. Col. Edward V. Richardson replaced Col. Lee as Chief of Staff.

By this time, HQ HANG staff included Sergeants Richard Fukui, Vivian Toma, Blossom Lee and Airman Pearl Duy, the first Active Guard/Reserve (AGR) appointee for the headquarters.



1983: Full-time staff takes a break from their recruiting discussion. Pictured left to right are Brig. Gen. Richardson, Commander; CMSgt. Charles Tasaka, Sgt. Major; Lt. Col. Raymond Moriguchi, Exec. Spt. Officer; and SMSgt. Richard Fukui, Senior Recruiter.

In January, 1979, CMSgt. Harry Awana became the first Senior Enlisted Advisor for HANG. He was followed by SMSgt. George Ah Chong, SMSgt. Kenneth Kiyabu, and the current advisor, SMSgt. Arthur Lee. A Thanksgiving bonus arrived at HQ HANG this year in the form of a quiet and subdued Ann Tsuji.

Under General Ishimoto's direction in 1979, the HQ HANG staff developed the first Five-Year Defense Plan which later became known as the HANG Planning and Scheduling Document. Capt. Jamie Bury, the first Operations and Training Staff Officer for the headquarters, was instrumental in drafting the plan.

Sgt. Major (CMSgt.) Chee retired in 1981 and was replaced by CMSgt. Charles Tasaka.

In early 1982, General U. Ishimoto was appointed as Adjutant General and Colonel Richardson soon thereafter was named as Commander HANG. During this period, considerable time was devoted with Hawaii's congressional delegation to get the Hawaii Region Operations Control Center



1985: Headquarters staff heads for ARPC, Denver, CO, for cross training. TSgt. Joyce Akagi (seating), SSgt. Dennis Takashita and TSgt. Vivian Toma (both standing) prepare to depart on a KC-135 from Hickam.

(HIROCC) approved for Hawaii. This effort followed earlier work done by General Siefertmann, General Ishimoto and Colonel Mossman, who laid the basis for the HIROCC initiative. The center was finally approved in 1982 for construction and equipment purchase of more than \$12,000,000. The center was completed in 1984 and declared operational in June. Five months later, the unit successfully passed their ORI.

On Nov. 23, 1982, Hawaii was hit by Hurricane Iwa and HQ HANG became the control center to coordinate airlift support for relief to the island of Kauai. The HQ HANG staff manned the center on a 24-hour basis for several days and coordinated HANG and other military services' airlift support.

The year 1982 closed on a high note with the promotion of Col. Richardson to Brigadier General in the month of December. An appropriate Christmas surprise and deserving recognition for the current Commander of the HANG.

1983 began as a very ambitious year for this headquarters. Plans were developed by the HQ staff, 326AD, PACAF and NGB to work towards obtaining the F-15 (Eagles) for the HANG. Also, plans were formulated to upgrade the crowded facilities at Hickam for the two Groups (154th and 201st). Facilities at both groups were inadequate to accommodate the tremendous growth in strength.

General Richardson continued to bring HANG into the total force policy by expanding the participation of the active forces and NGB various HANG activities to include the HANG Commanders' Conference. This eventually resulted in the early selection of HANG for conversion to the F-15 from the F-4C, which was supported by PACAF and NGB.

Several new people could be found scurrying around the office in 1984, either arduously working or mischievously playing. The group was led by Lt. Col. Raymond Moriguchi, the new ESSO, and included Maj. Edwin Miyahira, Capt. Wayne Wakeman, Capt. Edwin Vincent, Lt. Stanley Osseman and SSgt. Dennis Takeshita. A year later, Capt. James Robinson and TSgt. Geri Heatherly joined the full-time staff. The current ESSO is Lt. Col. Michael Tice who came in May of 1986 from the 199 TFS.

One of General Richardson's primary management thrusts has centered on readiness. In this respect, he sought improvements in equipment, facilities, training and resources. At the end of 1985, the strength of the HANG was at 100.2% and retention at 84.8%. This reflected the efforts of SMSgt. Richard N. Fukui, MSgt. Iris Kabazawa, as well as our recruiters and commanders. The HANG Athletic Board, under the able leadership of TSgt. Bert Narita, continued to expand and improve the various sports programs for HANG personnel. The year began with the golf tournament in August and ended with two HANG teams participating in the

15 ABW's softball for the first time in many years.

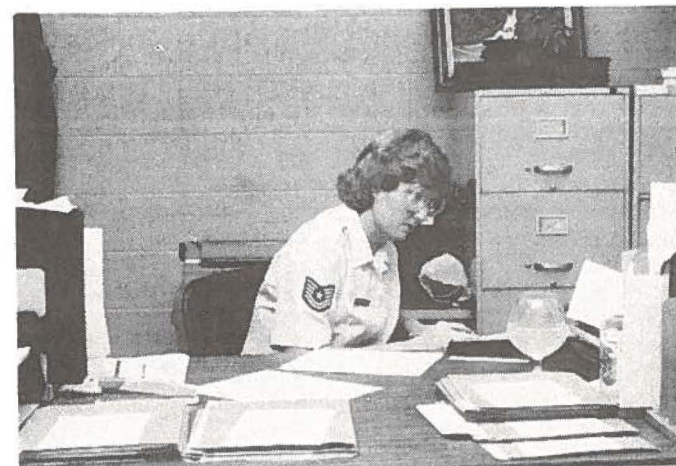
With roots going back to the late 1970s and early 1980s, some major initiatives started to materialize beginning in 1985 and visibly impacted the HANG. In 1985, we saw the arrival of the C-130A aircraft. The following year brought various HAMMERDOWN equipment from the 4 CISS in Korea to the 201 CCGP. The announcement of the replacement of the F-4C Phantom by the F-15A Eagle was also made in 1986. Another milestone in 1986 was the completion of the HANG Master Plan, a long-term facilities expansion plan. A new alert facility was completed in 1986. The HQ HANG staff also helped to improve training by obtaining the approval for more complex and challenging deployments for the 154 COMPG.

Over the past few years, General Richardson's emphasis on supervision and leadership led to the development of a professional and management development program, through seminars, to improve the quality of our supervisors and managers. The latest seminar was presented by Loren Lasher in October 1986. Earlier seminars were presented by Gordie Graham in 1984 and by Bob Moawad and Lou O'Neil in 1985. The results of this program will hopefully be seen over the next ten years in the supervisors' and managers' workplace and personal lives.

The changing missions and changing personnel over the past forty years have characterized the headquarters of the Hawaii Air National Guard. Many of the challenges of new construction, aircraft conversion, etc., have been met and solved by the dedication of many who have been assigned or attached to the headquarters to provide the needed expertise and leadership. As the HANG faces the next decade with the conversion to the F-15, HIROCC, and improved communications equipment, we will need to equip our supervisors and managers with skills to effectively manage an even more complex and technically demanding mission.



A1C (now SRA) George Stibbard, HQ HANG.



TSgt. Geri Heatherly hard at work at HQ HANG.

HANG 1986



Col. John S.W. Lee
Commander

154th Composite Group



official emblem

The 154th Composite Group (COMPG) is, just as its name implies, an intricate and complex composite of functions and activities that form the umbrella organization that oversees, maintains and supports the heart of the Hawaii National Guard's (HANG's) first line defense force. As you will see in the following sections of this book, each individual unit of the 154th Composite Group is capable and professional, and not only accomplishes each individual mission in an outstanding manner, but interacts with other Air National Guard units across the nation and the Pacific, exemplifying the dedication, perseverance and skill with which the Group and the HANG have become synonymous.

It is the task of the 154th COMPG's Commander and staff to coordinate these efforts into a tight, efficient and smooth running organization that accomplishes the mission directed from Headquarters Hawaii Air National Guard (HQ HANG). Considering the recognition afforded the Group in the last ten years, we have maintained the excellence set forth by our predecessors and evolved even further as an integral part of our nation's defense network.

The 154th COMPG came into being, as has all of the HANG as we know it today, through that evolutionary process. Col. John S.W. Lee assumed command of the Group on Aug. 1, 1978. Under this command are: a Deputy Commander for Operations; a Deputy Commander for Maintenance; a Deputy Commander for Resources; the 199th Tactical Fighter Squadron; the 154th Civil Engineering Squadron; the 154th Combat Support Squadron; the 154th Consolidated Aircraft Maintenance Squadron; the 154th Resource Management Squadron; the 154th Security Police Flight; the 154th Tactical Hospital; the 150th Aircraft Control and Warning Squadron; and the 169th Aircraft Control and Warning Squadron.

The 154th COMPG was initially organized as the 154th Fighter Group on Dec. 1, 1960, and consisted of six units. These units were: HQ 154 Fighter Group, 154th Air Base Squadron; 154th Materiel Squadron; 154th Consolidated Aircraft Maintenance Squadron; 199th Fighter Interceptor Squadron; and 154th USAF Dispensary. Its first Commander was Lt. Col. Robert B. Maguire. Col. James R. Ashford assumed command on Oct. 1, 1962 and served until August 1978.

The Group saw the transition of the F-86L ALL WEATHER INTERCEPTOR to the F-102 DELTA DAGGER in 1961 and the F-102 to the F-4C PHANTOM in 1976. The transition planning is already well under way in preparation for the arrival of the F-15 EAGLE in 1987.

Major unit deployments have included participation in Commando Road in Guam, Combat Sage and Cope Thunder at Clark Air Base and Cope North at Misawa, Japan. From Mar. 6-20, 1982, the 154th COMPG, with a six F-4Cs, participated in its first Red/Green Flag 82-03 as aggressor forces.

HANG 1986

A high point in the 154 COMPG achievement was duly recognized by the presentation by Lt. Gen. Hughes of the Air Force Outstanding Unit Award (AFOUA) on Mar. 1, 1980. The Pacific Air Forces (PACAF) Operational Readiness Inspection (ORI) conducted on Nov. 7, 1982 and Dec. 4-5, 1982 and the Management Effectiveness Inspection (MEI) during the Dec. 4-10, 1982 period resulted in a high ratings for both inspections.

During Mar. 8-21, 1983, the 154 COMPG participated in its initial Team Spirit staging out of Kunsan, Korea, with six F-4Cs and 115 personnel at the three Korean GCI sites. At the 169th AC&W Sq. location, Hawaii's air defense radar technology was revamped with computerized displays and instant information which has enabled Hawaii's air defense system to better respond to any emergency. The Hawaii, Region Operational Control Center (HIROCC) automated system became fully operational in the summer of 1984. At the National Guard Association of the United States (NGAUS) convention in 1983, the Composite Group was honored with the award of the Distinguished Flying Unit Plaque for 1982.

In Fiscal Year (FY) '84, the 154 COMPG deployed to Guam for a mini-air defense exercise in January, and deployed with six F-4Cs to Holloman AFB, New Mexico in April to utilize the ACMI range against the 7th TFS F-15s. Beginning in November and completed by Dec. 6, 1984 the PACAF IG staff conducted an intensive ORI and MEI of the 154 COMPG. The unit again achieved high ratings for both the ORI and MEI phases.

In January, 1985, the 154 COMPG hosted an ambitious Composite Strike Force Exercise which featured eight F-15s from Holloman to add to the usual participants in our Sentry Tigre exercise. In March, the 154 COMPG was contingent to participate in the Support Services exchange deployment with the Portland ANG. And in March the Composite Group was notified that the 154th COMPG had been selected for the Air Force Outstanding Unit Award for CYs 82 and 83. During May 85, the 154 COMPG deployed eight F-4C aircraft and 168 personnel to Clark AB to participate in Combat Sage and Cope Thunder activities. The intensive weapons and tactics training was highlighted by the first AIM-9L ever fired by an operational F4 unit. At the Air National Guard Association Conference held in September 1985, the 154 Composite Group garnered six awards which included the Distinguished Flying Unit plaque, the Mission Support plaque (169 ACWS) and four safety awards.

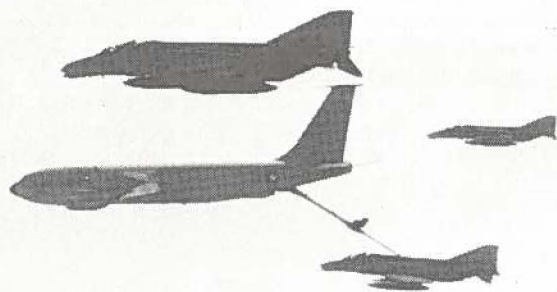
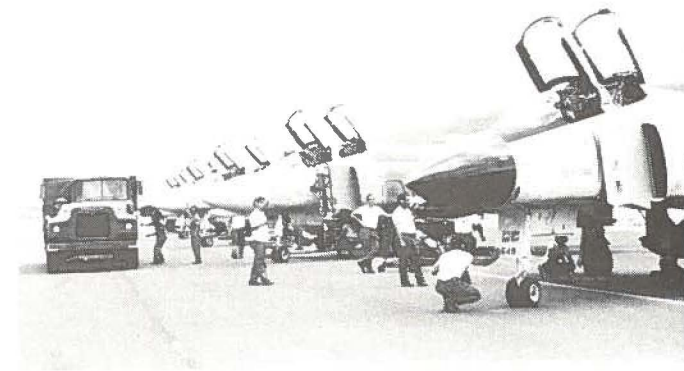
In 1986, the Group continued to move onward and outward, including: Sentry Tigre with Arizona ANG A-7s and the deployment of ten F-4Cs to Guam for a two-week deployment in January; AC&W support of Team Spirit; HANG's hosting of the 150th TFG (New Mexico) for a

Disimilar Aircraft Combat Tactics (DACT) exercise in March; HANG's hosting of the 140th TFG (Colorado) for a DACT exercise in April; that same month, the 154th Civil Engineering Squadron deployed to Clark Air Base in the Republic of the Philippines; in May, the 154th deployed to the Red Flag exercise (a major exercise) at Nellis Air Force Base, Nevada; and the deployment of the 154th Tactical Hospital to Elmendorf AFB, Alaska.

We've come a long way since 1946. We can now go farther and faster than we could way back then. And we have carved



The 154th Composite Group oversees and supports our combat fighter force.



F-4s take us to new frontiers thanks to air-refueling capability, as shown in this operation with a KC-135.



The 154 Composite Group has received the distinguished honor of having received the USAF Outstanding Unit Award, twice!

a distinguished niche in the edifice of time and the annals of military history. We have seen changes that, in 1946, would have boggled the minds of those who first called themselves members of the Hawaii Air National Guard. But one ingredient remains the same—people. The blood, sweat and tears of our people have carried us forward and the vision of our leaders have given us direction. No one knows where we will be forty years from now. But for the moment we can pause and reflect on where we've been and where we are and honestly say that we have continued the dream and maintained HANG's Tradition of Excellence.



The nerve center of the 199th Tactical Fighter Squadron, the command and control center.



TENT CITY! This was home for some of those members of the 154 Composite Group that participated in a live gunfire exercise at PMRF Barking Sands, Kauai in 1985.



Lt. Col. James S. Haruguchi
Commander

199th Tactical Fighter Squadron



The 199th Tactical Fighter Squadron was formed on Sept. 10, 1946 as the Hawaii National Guard was re-established following the end of WWII. Initially assigned P-47N "Thunderbolts" and B-26C "Invaders", we entered the jet age with F-86E "Sabres jets" in 1954, F-86Ls followed in 1958, and supersonic F-102 "Delta Daggers" in 1961.

The F-4C "Phantom" era began in 1976 when members of the "Laguna Cove Opihi Eating Team" departed TDY to Luke AFB, Arizona, and George AFB, California for transition training. While at Luke, the aircrews bought a mascot named "Jet Noise". It was not a pet, but rather a trusty steed in the form of a Chevy sedan that provided transportation for weary aircrews between the squadron, BOQ, and the O'Club where "all we did was eat."

Air defense alert activities for the unit began on July 1, 1956 with two aircraft on "day alert." On Oct. 23, 1958, a 24 hour air defense alert commitment was assigned the unit. Notable is the fact that the alert commitment has been maintained throughout all of the unit's aircraft conversions, without interruption and without assistance from other flying units, a fact the unit is proud of.

The 199 TFS was the first Air National Guard squadron to perform air defense alert with the F-4C. Realizing that the capabilities of the Phantom allowed for more than just an air defense mission in Hawaii, the unit pursued an enhancement of the mission. With approval from PACAF and the NGB, the unit changed from a purely air defense to a more challenging role of air superiority. This mission change made the 199th the first Air National Guard squadron to be assigned that role.

The opening of 1977 was preceded by a six-month long "opportunity to excel" as the unit transitioned from F-102 to the F-4C. By January the F-4Cs were firmly in place after having replaced the F-102 on alert for the previous three months. Squadron operations began settling down as experience was gained in the F-4. There was still much work ahead, as a large percentage of the aircrews were not yet qualified as mission ready.

Being a two-seat aircraft, the F-4 required Weapon Systems Officers (WSO's), as well as pilots. This created a need to double everything, including beer and pupus for post UTA festivities. Much of the credit for development of the F-4 program goes to our initial cadre of WSO's, including "imports" Maj. Jack Salter, Phil Matsusevic, Jim Magee, Capt. Dick Hastings, Ray Putnam, Gar Pilgrim, Bill Jelks, and finally Lieutenant Mike Dudley. Our "local boys" were Capt. Mel Matsui, Dave Nishioka, Lieutenants Clyde Torigoe, Dale Tengan, Gordon Fujimoto, and Joe Akim.

Months of hardwork paid off over the September UTA when we successfully completed our first F-4 Operational Readiness Inspection (ORI). Flying scores were satisfactory; Life Support, under the direction of MSgt. Howard Hironaka and "company" received an "Outstanding", a tradition that

HANG 1986

still continues.

The year closed with an Aloha Party, held at Maj. Hal "Shaka" Schatz's house for departing F-102 pilots, Maj. Tom "Bull" Haine, George Joy, Mel Souza, George "Dusty" Kuroda, and John King. Capt. Kimo Moncrief was also there trying to perfect his infamous dance step.

Since the transition into the F-4 two years prior, the unit would have to finally prove itself in 1978. The HANG sent four F-4Cs to participate in a Combat Sage exercise. It was the HANG's first major deployment in its 32 year history. Combat Sage, which takes place at Clark AB, in the Philippines, tests the capabilities of aircrews as they attempt to shoot down drones with their radar-guided Sparrow and heat-seeking Sidewinder missiles. Several innovative techniques in positioning aircraft for missile employment were used.

Also during 1978, the air combat training program was initiated and met with much aircrew enthusiasm. The program trains and exposes the crews to basic and advanced combat maneuvering. Arms and hands now began to flail around the briefing rooms as crews attempted to reconstruct their "side" of the "dogfight" missions and prove how "they" won the fight.

We patted ourselves on the back for being named the HANG's Outstanding Unit for 1978.

Lt. Col. Kurt and Claire Johnson hosted the annual squadron picnic. Afternoon activities included diving for Lt. Col. Edward Richardson's outboard motor.

Long time squadron secretary, Alice Doyle, retired. Operations hasn't been the same since and she's still missed and fondly remembered.

On Sept. 1, Lt. Col. Lawrence "Cabby" Cabrinha took command of the squadron from Lt. Col. Edward "Rick" Richardson when he was promoted to the Chief of Staff position at HQ HANG.

The accumulation of many thousands of hours of training needed to refine skills and excel in the complex F-4, would be put to the test in 1979.

The first test came in June. Six F-4s roared off from Hickam AFB on June 1st headed to Guam. Phase I took place at Anderson AFB, against the Strategic Air Command's B-52 bomber force stationed there. Our aircrews and radar controllers defended Guam from being attacked by the B-52s. The missions involved training in electronic warfare and air defense exercises. The results were outstanding since every bomber was neutralized prior to target.

Phase 2 began on the start of the second week. The aircrews on Guam were replaced by a new set of aircrews from home. This new set then flew the aircraft to Clark AB, for Combat Sage/Cope Thunder Exercises. Following the missile firings in Combat Sage, the aircrews participated in Cope Thunder which involved advanced air combat tactics against the Aggressor F-5s stationed at Clark. The smaller,

more agile F-5 simulated Russian aircraft and combat tactics. Fighting against them proved very challenging and provided excellent training for our aircrews and radar controllers on not only how to hold their own, but ultimately, on how to prevail over most modern Soviet interceptors and tactical fighters.

The second test came later in the year with another Operational Readiness Inspection (ORI). Early morning recalls, detailed briefings and debriefings, along with simulated air defense and air combat missions provided a very tense and demanding environment for aircrews. The results: "Outstanding". It was considered by many to be the best inspection in the history of the 199th. SMSgt. George Sato and MSgt. Henry Yamaguchi and their dedicated operations and administrative technicians played a key role in achieving this rating. They're the unsung heroes of the 199th.

The 199th's accomplishments of 1980, along with its long and distinguished record, provided the impetus in PACAF's decision to nominate the unit for the coveted Hughes Trophy for FY 80. By providing active air defense alert for the islands since 1956 and demonstrating combat capability during their last ORI, "the unit reflected the highest standards of professionalism and combat readiness." (Hoyt S. Vandenberg, Jr., Maj. Gen., PACAF/Vice Commander in Chief.)

A deployment to Guam and to Japan for exercise Cope North highlighted the year. The HANG trained with the Japan Air Self Defense Forces (JASDF) at Misawa AB, who flew their F-1s, F-104s and F-4EJ aircraft. We considered the flying and hospitality absolutely great and hosted a luau to reciprocate. Lt. Col. Joe "Whadayameanmyeyebrowsare-gone" Ah New utilized the latest in imu firelighting techniques.

Throughout the year, the unit flew air combat training sorties against several visiting units. The A-7s from the ANG Weapons School at Tucson, Arizona, were to become welcome annual visitors for the Sentry Tigre exercise.

Maj. Gar Pillgrim produced, directed and marketed a series of HANG recruiting advertisements that were shown on all local television networks. The ads were tremendously successful and contributed directly to the HANG's recruiting efforts and manning strength of 98 percent.

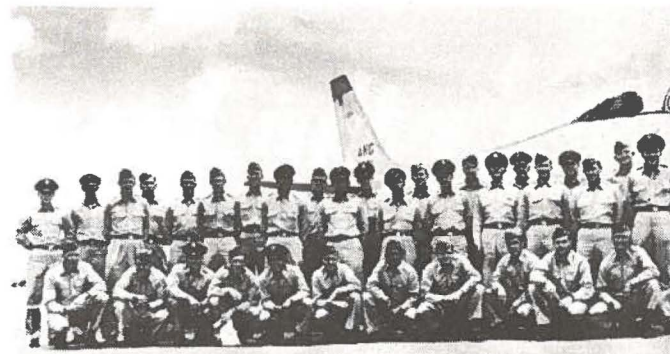
The intensity of our training scenarios increased throughout 1981. The A-7s from Tuscon returned, as did F-14s from Navy carriers and Marine A-4s and F-4s. A new training experience took place when we were able to work with the Air Force's latest version of the C-135, the E-3A Airborne Warning and Control Aircraft (AWACS).

Our deployment for the year took us back to the Philippines for Combat Sage and Cope Thunder. Aircraft radar and missile launch systems and aircrew performance were all rated at 100 percent during the Combat Sage events. Lt. Art "Sky" Hicks and Maj. Bill "Willie" Jelks scored a direct hit on a drone. The Cope Thunder sorties were flown in support of the F-5 Aggressors.

The television program MAGNUM P.I. requested assistance in filming various sequences for an upcoming show. Lt. Col. Kurt Johnson flew an F-4, which was painted up like a Russian MIG, and landed at Hickam simulating a defecting Russian pilot.

In March 1982, the unit flew six F-4s to Nellis AFB, Nevada, to participate in its first Red Flag exercise. Although similar to Cope Thunder, Red Flag takes place in a desert environment rather than a jungle and ocean. It's considered the premiere of all air combat training exercises. Previous experiences in the Philippines enabled the HANG to perform admirably in the scenarios. Maj. Gerrit "Oz" Osborne and Capt. Fred "Tri Fox" Fogel also participated in a survival exercise in the desert. Results were "classified".

Also in March, we supported the Defense Nuclear Agency's Project ALPS-Air Launched Probe System. Cpts. Ed "Easy" Pickering and Mick Melich flew a specially modified F-4 to extremely high altitudes in order to launch a single



1954—From L-R: unknown, Frank Carlos, O'Hara, Russell Smith, Letwell "Paul" Duvachelle, unknown, Walter Judd, Jarrett Carr, Olds, Kumulike, George Harris, unknown, Soren, unknown, Evans, Chambless, unknown, Smith, Fisher, Frank Harlocker, Valentine Siefertman, Barnett, *Kneeling:* Harr, Harrison, unknown, Lee, unknown, Lee, George Duncan, Ludloff, Flake, Dau.



199th TFS pilots in the Philippines for Cope Thunder/Combat Sage exercise, 1985.



HANG's F-4s sport three different paint schemes to aid in dissimilar aircraft training.

rocket probe. The measurements taken from the launch were beneficial to future ballistic missile defense development.

Goodbyes were said to Lt. Col. Gerald "Jerry" Sada who retired in July and to Maj. Bill "Willie" Jelks who returned to active duty life in the Air Force. The "Sada Salute" was introduced at their Aloha party. The "real" Lt. Col. Mike "Boss" Tice surfaced as master of ceremonies for the event.

Our deployment for 1983 was to Kunsan AB, Korea for Team Spirit. The scenarios flown were as realistic as can be and the experience gained was immeasurable in improving the air superiority level of our aircrews. Col. John "Saigon" Lee, Lt. Col. Tai Sung "Mynah" Hong, and Capt. Darryl "Yobo" Chung felt right at home although there was one tense moment for one of them on the radio. "Taegu Hi, Taegu Hi, this is Pelon 01, How do you read? Over."

A deployment to Guam for Cope Road air defense training against the B-52s kicked off 1984. This was followed by another deployment to Holloman AFB, New Mexico. We trained against F-15s and were able to use the Air Combat Maneuvering Instrumentation (ACMI) equipment for the first time. The evidence was now irrefutable. We were great!! The ACMI allows aircrews to review their flights and to see exactly what happened and what lessons can be learned. The training gained from this instrument alone is immeasurable to aircrews.

Alaskan Air Command F-15s deployed to Hawaii for training with us. We were quickly learning the versatility of the modern F-15. Its advanced radar system, outstanding maneuverability and acceleration made it a more than challenging opponent.

Lt. Col. William "Bill" Spillane took command of the 199th as Lt. Col. Joseph Ah New, Jr., retired. "Primo Joe" flew his last flight in the summer of '84 ending a long flying career. His leadership and fellowship will not be forgotten. Retiring with Joe was Col. Kurt Johnson. Kurt had joined the HANG in the early 1950s after serving in the Air Force in the Korean War where he achieved "MIG Killer" status. Both Joe's and Kurt's commitment and inspiration to the unit had earned them the respect and admiration from their fellow aviators. They remain true images of the Hawaiian warrior and fighter pilots "extraordinaire." Besides leaving behind a legacy, Kurt also left behind his son, Lt. Garro "Rojo" Johnson. A recent returnee from F-4 training, Ro and his Dad became the first father and son aircrew team in HANG history. Ro flew in formation with his dad during Kurt's last flyover. It was truly an emotional and memorable salute to a great guardsman.

The old but venerable C-7 Caribou was finally replaced by a C-130A Hercules. The increased capabilities of the C-130 were almost ten-fold. From C-7 cargo loads of 2,000 pounds and 20 passengers, it increased to 20,000 pounds and 75 passengers in the C-130. Maj. Eddie Miyahira was chosen to be head honcho of the C-130 program.

The annual Sentry Tigre exercise started 1985 off. The A-7s from the Arizona ANG, and the F-15s from Holloman AFB, New Mexico, deployed to Hawaii for two weeks of flying, sunshine and beaches. The training for all the participants was outstanding. In the end, we fared well and all had a great time at the Mexican cookout on the last day. We could all attest to that the next morning.

In May the squadron left for another the Combat Sage/Cope Thunder exercise. This deployment was similar to the previous ones with one exception. This deployment had more lieutenants flying than any other deployment in the past. The senior officers had to pay close attention to details due to the junior officer's limited experience, but in the end it was the lieutenants who had to repeatedly save their leaders in the "dogfights" both in the air and on the ground. During the Combat Sage phase, the squadron became the first Air Force F-4 unit to employ the latest in sidewinder missiles, the Aim-9 Lima. We were credited with 100 percent hits on all drones. Overall, the flying was great, and so were the "Green Bean Jeepnie Sweeps". We frequented familiar haunts and spread goodwill throughout Angeles City; specially Maj.

Mick "Allnight" Melich and Capt. Wayne "Takeitalloff" Wakeman.

The squadron guidon flag exchanged hands as Lt. Col. James S. "Gooch" Haruguchi took command of the 199th from Colonel Spillane in December 1985. At the promotion party, Bill promised not to brief 3 bar scan techniques to the WSOs anymore.

The old alert pad underwent a complete overhaul. The crew's quarters between the hangers was demolished and work started on a new two story, fully air-conditioned facility. We temporarily moved over to the Blue Eagle facility across the taxiway and were quickly spoiled by the single rooms. Many had difficulty sleeping at night without the accustomed background snoring.

In January 1986, the Alert crews moved back to their newly renovated facility. The "Pad" now boasts new carpeting, air conditioning, a washer and dryer, recreation room, and, most important of all, picture windows to check the weather. The old pad didn't have windows so the new addition was welcomed with open arms. The aircrews finally had an Alert Pad they could be proud of.

A-7s from the New Mexico ANG at Kirkland AFB, deployed early in the year to Hawaii. They were followed later by the A-7s from the Colorado ANG. As in the past, both units provided the HANG invaluable training in the combat arena.

A solemn "missing man" flyover was performed over the Punchbowl's National Memorial Cemetery of the Pacific in honor of Challenger Astronaut, Lt. Col. Ellison Onizuka. He was a Hawaii-born aircrew member who was killed in the Space Shuttle accident in January. The squadron, like all of Hawaii was touched by the tragic loss.

The squadron's deployment this year took our aircrews to another Red Flag exercise in May. This deployment would be the last for our F-4Cs as news conversion to F-15s was announced a few months earlier.

The "Eagles" would arrive in 1987 to replace our aging F-4C "Phantoms". The news caused a lot of happy faces, but also a lot of sad ones, especially for the WSOs. Moves will have to be made to other non-flying jobs since the F-15 is a single seat fighter. As we all look forward, the F-15s technology will not only increase our capability to fulfill our mission, but will also be a tremendous milestone in the annals of HANG history and a nice way to start our next 40 years!

We will not forget the "Phantom Era" and the WSOs who have served with us over the years and contributed so much. For those still with us, as well as to those who have left, the 199th takes great pride in recognizing you and saying Mahalo, Aloha, and "Nana Okole".

- | | |
|------------------------|-----------------------|
| Lt. Col. Dick Hastings | Capt. Gordon Fujimoto |
| Lt. Col. Jack Salter | Capt. Carl Gallegos |
| Maj. Jamie Bury | Capt. Ron Gardner |
| Maj. Roy Cornella | Capt. Dave Gilbert |
| Maj. Rich Dove | Capt. Mike Hilliard |
| Maj. Mike Dudley | Capt. Pete Heeter |
| Maj. Fred Fogel | Capt. Mark Johnson |
| Maj. Bill Jelks | Capt. Kevin Kamita |
| Maj. Jim MaGee | Capt. Herman Lau |
| Maj. Mel Matsui | Capt. Chuck Manley |
| Maj. Phil Matusevic | Capt. Dave Morais |
| Maj. Mick Melich | Capt. Stan Osserman |
| Maj. Jeff Nishimura | Capt. Bill Patronis |
| Maj. Dave Nishioka | Capt. Chuck Pribyl |
| Maj. Gar Pillgrim | Capt. Jim Royer |
| Maj. Ray Putnam | Capt. Biff Strom |
| Maj. Willson Sakai | Capt. Dale Tengan |
| Maj. Nick Sporik | Capt. Clyde Torigoe |
| Maj. Brad Stonesifer | Capt. Mike Troncoso |
| Capt. Joe Akim | Capt. Skip Vincent |
| Capt. Bob Brown | 1Lt. Steve Coonan |
| Capt. J.C. Clark | 1Lt. John Higgins |
| Capt. Omar Faufata | 1Lt. Scott Hoadley |
| Capt. Harold Fujii | 1Lt. Dal Wong |



Lt. Col. Norman C. Ault, Jr.
Commander

154th CAM Squadron



official emblem

Looking back in time, and examining the illustrious history of the 154th Consolidated Aircraft Maintenance Squadron, it is no wonder, and quite fitting, that the Squadron motto "NO KA OI" (THE BEST) was chosen. The history of the 154th CAM Sq. goes as far back as the initial organization of the 199th Fighter Squadron (FS) on Sept. 10, 1946.

The 154th CAM Sq. was officially recognized in December 1960 when the 154th Fighter Group (FG) was officially organized. Let's take a look in history, back to when the fledgling and newly formed Hawaii National Guard's 199th FS.

It was Aug. 3, 1946 when Governor Ingram Stainback issued an Executive Order re-establishing the Hawaii Air National Guard (HANG). The new Adjutant General, Brig. Gen. Fred McKinney, formally accepted four units of the Army Air Force: the 199th FS, 199 Utility Flight, Detachment C, 199th Air Service Group (Ftr), and the 199th Weather Station (Type A). Lt. Col. Walter Dillingham was the first commanding officer of the 199th FS, and he was soon succeeded by Maj. William Morgan. Maj. Morgan resigned and was replaced by Capt. Frank Harlocker.

Maj. William Nolan assumed command of the 199th FS on Dec. 18, 1946. On Mar. 19, 1947, the first assigned aircraft of the 199th FS, B-26C no. 433 was received. By the end of the year, two AT-6s, four B-26s, one C-46, and two L-5s had been delivered to the fledgling units. Bellows Field was made available for the exclusive use of the 199th FS, however, the maintenance of the aircraft was hampered due to a tremendous lack of parts and equipment. Since Hickam Field was where most of the aircraft flying activity was located for the Army, Air Force and civilian aircraft, "scrounge runs" by "Nolan and the Forty Thieves" was a common practice and occurrence, with the C-46 being used as a getaway vehicle on those "raids".

Maintenance workstands usually consisted of 55 gallon drums, and flight line personnel usually pulled double duty working on the fire and crash truck during most aircraft landings.

In July 1947, the unit received its first P-47N Thunderbolt (JUG). The 7th Air Force then approved a change of station for the HANG to Hickam Field, mainly due to a lack of space for all the aircraft assigned to the unit. Good news was received when the HANG became a reserve component of the newly constituted United States Air Force on Sept. 18, 1947. The HANG completed its move to Hickam Field on Oct. 28 that same year, occupying two open-ended Butler hangars and a miscellaneous assortment of wooden World War II shacks. Possessed aircraft at the time of the move was twelve P-47Ns, two C-46Fs, two B-26Cs, five AT-6s and two L-5Es.

All P-47N "Jugs" were grounded in October and November

1947 due to Fuel Selector Valve Seal Leaks. Because of insufficient funds, parts and supplies were normally back-ordered 90 to 120 days. "Nolan and his Forty Thieves" were again called upon to solve the fiscal and logistics problems, and came through with the "acquisition" of large quantities of "surplus" property from Navy "sources".

The HANG became responsible for the reclamation of 79 P-47Ns of the inactivated 81st Fighter Group from Wheeler AFB. Airframes and engines of the 81st Fighter Group soon acquired the numbers and plates of the HANG. Parts for the aircraft were good, enough for a "150 year" stock level. Parts were so plentiful that when a mechanic needed a particular type, he had to go through a warehouse piled with aircraft parts to find the right part for job repair. The "P's" were dropped from all fighter aircraft and replaced with "F's" for fighters and by the end of 1949, aircraft mechanics had to maintain twenty-seven F-47Ns, four TB-26s, five T-6Ds, and two C-46Fs.

By the end of Fiscal Year (FY) 1952, the number of aircraft possessed had decreased to twenty-four F-47Ns, three TB-26s, two C-47As, and two T-6Ds. This was mainly due to fiscal constraints. The number of possessed aircraft remained about the same for the next couple of years. Then came word in FY 1954 that the HANG would be converting from the JUGs to the F-86E SABRE JET and the T-33 T-Birds. The F-86s are the same jets which gained fighter fame in the Korean War. With the arrival of the first SABRE JETS on Feb. 10, 1954, the HANG entered the JET AGE, and the SABRE JET would soon become a prelude to bigger and better things to come.

With the arrival of the F-86E came the realization that all would not be the same for the HANG. In early FY 1955, Maj. Gen. Sory Smith, Commander PACAF (Rear), proposed the HANG provide an active air defense for the entire state which would require having aircraft on Alert status. After receiving approval from the U.S. Air Force (USAF) and the National Guard Bureau (NGB), the HANG was asked to participate in the active air defense program effective July 1, 1956. This commitment required two combat-ready pilots and two fully armed SABRE JETS on five-minute alert during daylight hours. Thus marked another milestone and another "first" for the HANG. The unit had a total of 15 F-86 aircraft possessed to accomplish this new mission. The F-86E was an excellent aircraft for daytime duties. The mechanics who worked the airplane got to know the aircraft inside and out. However, on Feb. 5, 1958, the HANG was authorized to receive the newer F-86L All Weather Fighter Interceptor and with it the responsibility for a 24-hour active air defense commitment. On May 1, 1958, the HANG commenced sunrise to sunset air defense duties with the "Ls", and the "Es" were phased out by May 30, 1958. In October 1958, the 199th FS went on 24-hour active air defense alert status. Thus



WE'VE GOT TO GET A BETTER TROOP TRANSPORT! L-R: unknown, Robert Choi, unknown, Harold Mattos, Valentine Siefertmann.



Benjamin Goo, Harold Mattos and Robert Choi were among those recognized at CAMRON's annual Recognition Night banquets.

marked the beginning of another era for HANG.

Dec. 1, 1960 marked another milestone as the 154th Consolidated Aircraft Maintenance Squadron (CAM Sq. or CAMRON) was born, commanded by Maj. Jarrett Carr. For the men who worked to maintain the aircraft in an outstanding operational ready status, this organizational structure provided a new awareness and sense of pride. Just who were the men who came through time and time again during those early years of learning and scrounging? The names are many but they set the groundwork and tradition for bigger and better things that were to come. Men such as Harold Mattos, Norman Ault, Allen Mizumoto, Robert Choi, Frank Farley, George Meyer, Gordon Martin, John Beaumont, Fred Rittmeister, Anthony Kim, George Joaquin, and Clem Crowell worked long, tireless hours, and for them, to be a part of the CAM Squadron would give them a new sense of pride. No sooner was the organization drawn up that the first F-102A Convair Delta Dagger was received on Dec. 5, 1960. Thus began the long and time consuming process that an aircraft conversion entails. As was the tradition, the men of the 154 CAM Sq. pulled together and were ready for the "Deuces".

Lt. Col. George Duncan assumed command of the 154th CAMRON July 1, 1962 and continued the "can do" tradition of the Squadron as the maintenance personnel continued to learn about the "DEUCE" and what makes it fly. On July 20, 1963, CAMRON was deactivated and became a part of the 154th Materiel Squadron (MS). Maj. Harold Mattos, who had been a mechanic in the "early days" working as a Master Sergeant and had worked himself up to Chief Warrant Officer 2, then to a direct commission as Captain in April 1961, assumed command of the 154th MS on May 11, 1965.

On December 1, 1965, the 154th Materiel Squadron was inactivated and the 154th Consolidated Aircraft Maintenance Squadron was re-activated. Maj. Harold Mattos was appointed squadron commander, with a fleet of 20 DEUCES possessed. By May 1966, CAMRON had received the last five aircraft they were authorized, bringing the total possessed to 25 aircraft.

Maj. Norman Ault assumed command of the CAMRON on Jan. 21, 1974 due to the retirement of Lt. Col. Harold Mattos. After fifteen long years "caring for" the DEUCES, the CAMRON again began the arduous task of converting to the newer F-4C Phantom, the conversion being the result of the Department of Defense directive called Total Force. The first "Phantom" arrived in October 1975, and a new chapter for the 154th CAM Sq. was to be added to its already overflowing book of accomplishments. An entirely new concept of aircraft maintenance and superiority had to be "taught" to the

CAMRON maintenance personnel.

It should be noted that Lt. Col. Mattos and Maj. Ault had actually "come through the ranks" of the enlisted and become Squadron Commanders of CAMRON. The "can do" attitude of the 154th CAM Sq. was evident as they learned about the aircraft and at the same time flew all required missions. The dedication of the men and women of the 154th CAM Sq. was instrumental in the unit attaining an "excellent" rating during Operational Readiness (ORI) and Management Effectiveness Inspections (MEI) since converting to the F-4C aircraft. Deployments to take part in Team Spirit (Korea), Cope Road (Guam), Combat Sage and Cope Thunder (Philippines), and Red Flag (Nellis AFB) are just a few of the exercises CAMRON has participated in, providing aircraft maintenance in support of the flying requirements needed for those exercises. The men and women of the 154th CAM Sq. are certainly proud of the tradition set forth by the "old-timers" of years past, and they continue to carry that heritage in grand fashion. The "feather in the cap" happened when the 154th CAM Sq. was subjected to what would be its last ORI and MEI in November and December 1984 with the F-4C aircraft. All CAMRON maintenance branches received an "outstanding" rating which gave the men and women of the unit another "first", an overall unit rating of "outstanding".

The fruits of their labor have been realized with the announcement of yet another conversion, this time to the F-15 Eagle. The training for the personnel will be intense, much more so than in the past. The unit must still maintain its Alert Status during its conversion, and it must continue to provide the active air defense program it had provided since July 1956, over 30 years ago. Support aircraft have been upgraded and the unit presently maintains a C-130 aircraft which complements its 27 F-4C Phantom fighters.

The heritage and tradition of the 154th Consolidated Aircraft Maintenance Squadron had its start back in the early days of 1946. It had been nurtured over the years and came into its own in 1965 when the 154th CAM Sq. gained official recognition as a member of the 154th Fighter Interceptor Group (FIG), which later became known as the 154th Composite Group (COMPG).

Lt. Col. Norman Ault still commands the 154th CAM Sq. and he will be at the helm when this unit converts to the F-15 aircraft. Colonel Ault will be going through his second conversion as he was the commander when the unit converted from the F-102 Deuce to the F-4C Phantom.

As was mentioned at the beginning of this summary of the 154th CAM Sq., the unit motto is and always be "NO KA OI". The unit was proven time and time again that they're truly "THE BEST".



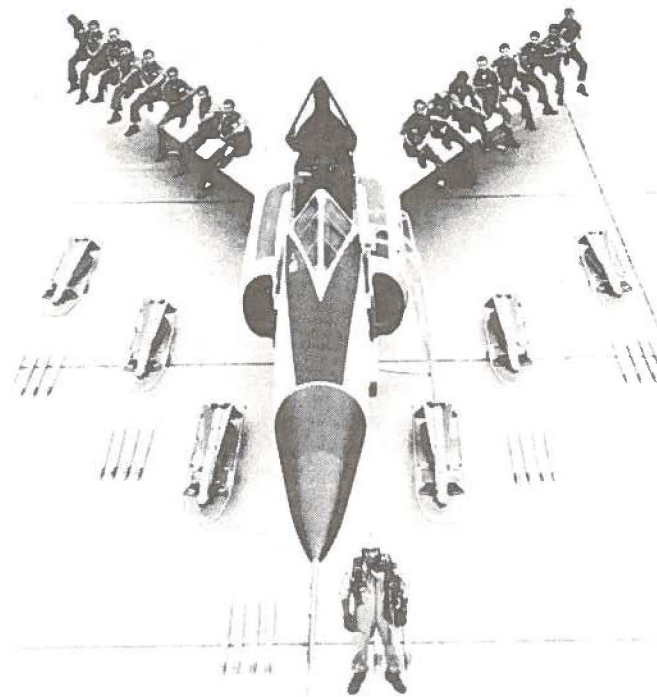
FAMILIAR FACES? Yes, that's Kenny Camara kneeling down under this "new" T-33, in 1953, with a few less stripes than he sports these days. And just in front of him is Art Lee also sporting a few less stripes. On the wing, with their backs to the camera, Russell Smith and James Ashford.



HOT SHOT! MSgt. "Big John" Beaumont (second from right), in 1956, placed second in the International .22 Caliber Rapid Fire at Camp Perry National Matches and then placed seventh, with the American Olympics Pistol Team, in competition with the World's finest shooters.



CAM Sq. members during Philippines deployment, 1985.



F-102. L-R: F. Farley, Yabui, Chang, Tokoro, T. Kawamoto, J. Aquino, R. Loo, N. Teves, P. Yonting, F. Rittmeister, Souza, D. Lee, T. Tsuruda, W. Yara, S. Salvador, Tony Kim.



This CAMRON worker really gets into his job.

HANG 1986



Lt. Col. James K. Iwamura
Commander

154th Resource Management Squadron



The 154th Resource Management Squadron (RMS) has come a long way from what it was when first conceived in the early 1960s. The unit members were assigned to the 154th Materiel Squadron (MS) under the command of Maj. Tyler M. Harr. The basic composition at that time consisted of maintenance and supply personnel but sometimes took on a catch all role as the "provisional Detachment" which was to remain at home station to "keep house" while the bulk of the HANG deployed.

On Dec. 1, 1965 the 154th MS was inactivated and the 154th Supply Squadron (SS) was activated under the command of Maj. Frank Carlos. The new squadron quickly developed a reputation and was called "gung-ho" or tigers by many.

On May 1, 1966 a Captain named Daniel K.C. Au took over the reins determined to make his mark. By October 1966, the Supply Squadron had achieved one year without a mission cancellation due to lack of supply parts. It did not stop there however, and when the unit was finally charged with a Not Operational Ready-Supply (NORS) delay in 1969 it had recorded an unbelievable record of 1266 days NORS free. This was the best record in the Air National Guard at the time and as far as we know has never been equalled since. In recognition for this outstanding achievement, the 154th Fighter Interceptor Group was presented the Pacific Air Forces Supply Outstanding Achievement Award for attainment of a zero NORS rate for nearly three and a half consecutive years.

The squadron got involved with other things such as hosting the Unalakleet Alaska students and providing members for the State Civil Disturbance Team. It went on trips to Barking Sands, Kauai and Kilauea Military Camp on the "Big Island". It recognized its outstanding personnel such as MSgt. Lino Fernandez of the Supply Warehouse Branch who won the 1967 HANG Senior NCO of the Year Award, A1C Alan Matsuoka of the Fuels Management Branch who received the 1971 HANG Outstanding Airman Award, and Maj. Gerald Morihara of the Supply Management Branch who received the James Y. Sato Memorial Award in 1972.

While all of this was going MSgt. Howard Okita got the inspiration for the squadron logo "HOOHOLOA" meaning "to give or provide what is desired". TSgt. Samuel Wong also found that to be a fitting title for the squadron's newspaper and began publishing stories and events of interest to those in the unit.

When Lt. Col. Au was moved up to Deputy Commander for Logistics in 1975, Maj. Gerald Morihara assumed the command. That year another "fast burner" by the name of Capt. James K. Iwamura demonstrated outstanding performance and service to the HANG and captured the James Y. Sato Memorial Award again for the squadron. Other faces started making news in the mid to late 1970s. Amn. Pamela

HANG 1986

Eiler became the squadron's first female enlistee in 1975. MSgt. Howard A. Okita won the HANG Outstanding Senior NCO of the Year Award in 1974 and so did SMSgt. Edward Schweitzer in 1976 and MSgt. Ronald K. Izutsu in 1977. Mechanization of the supply system changed to computerization and standardization using the UNIVAC 1050-II System. Community participation continued with members of the state civil disturbance team being called to State active duty to secure and search Oahu Prison. "Aloha Spirit" was generated when A1C Louise Sharpe embarked on a three week tour of Europe with the "Hauoli Dance Group", and other members of the squadron cross trained with the 152nd TRG of the Nevada Air National Guard.

The end of the decade (1979) saw major changes with Lt. Col. Richard Furuno taking over command of the newly created 154th Resource Management Squadron (RMS). Under his scope of control came the Comptroller, Transportation, Base Procurement, Logistic Plans, and Supply. With this new strength came new roles. Although specialized cross training trips continued, more mission oriented deployments were made without fighter aircraft and key elements of the 154th Composite Group (COMPG).

Upon the retirement of Lt Col Furuno in 1981, command of the Resource Management Squadron fell to Lt Col James K. Iwamura who had distinguished himself with his positive "can do" attitude. Lt Col Iwamura's leadership permeated throughout the squadron and motivated TSgt Samuel Wong to design the current logo of the Hawaiian God Lono (God of peace, fertility, and abundance) catching a wave (symbolizing the vast Pacific Ocean area we cover) and holding thunderbolts in both hands (showing we have our hands full in providing support to everyone). SSgt Jan DeRego was also inspired to rejuvenate the squadron newspaper and call it "I Kaika O'RMS" meaning "the strength of RMS".

Although the workload in the squadron was often extremely heavy due to various exercises (Cope North, Cope Thunder, Combat Sage, Project ALPS, Red Flag, Cope Road, Team Spirit, "Gunfire", and Sentry Aloha to name a few) its members still found time to stay involved with off duty activities which reflected credit on the Hawaii Air National Guard. For example SSgt Milton Watkins and SSgt Sherry Torres spear headed a project to provide food, clothing, and essentials to Hawaii's "street people" through the Institution for Human Services. SSgt Jan DeRego ran for and was elected as President of the Hawaii National Guard Enlisted Association. TSgt Rose Vendiola, SSgt Pam Guieb, and SSgt Louise Lum somehow still manage time for their impression of Hawaiian Kelly Girls donating their services for HANG related entertainment and Aloha parties. Of course Lt Col Melvin Asai, SMSgt Howard Sato, and SMSgt Alan Matsuoka cannot be overlooked for their current parts in making

"mission impossibles" become possible. Neither can Msgt Kalfred Chang be forgotten for getting our HANG people wherever and whenever they need to be.

There are many other people in the squadron deserving of recognition for doing more than what the job calls for (such as SSgt. Michael Arakawa and MSgt. Harrison Iga who really are inventory management specialists but substitute as typists, drivers, and general "Jacks of all trades" for different occasions).

These people are all winners, and so it was rather symbolic that the teamwork and spirit of the squadron manifested itself as the RMS took the 1986 HANG Basketball Championship. This championship was taken with a wide cross section of people (Maj. Allen Shinsato, Comptroller; TSgt. Alvin Naumu, Material Storage & Distribution; SSgts Melvin Akamine, Military Pay, Anthony Denault, Special Assets, Michael Soares, Refueling, Alfred Valeros Jr., Refueling; Sgts



Group shot of RMS Squadron personnel with Commander, Lt. Col. Richard Furuno. Colonel Furuno retired from the HANG in 1981.



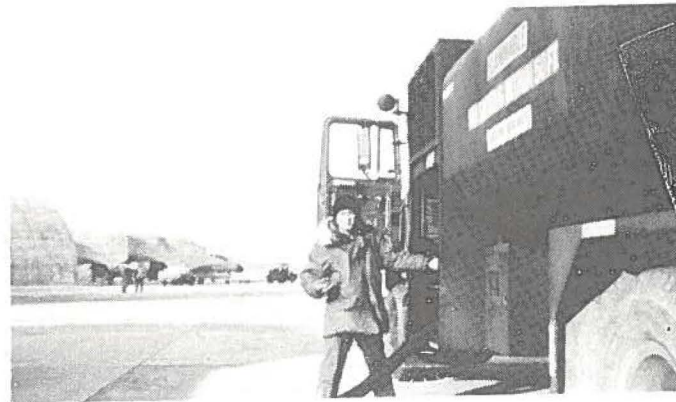
RMS members are constantly busy maintaining and documenting HANG resources. Workers here prepare a new storage area.



SSgt. Gilbert Kaneshiro, SMSgt. Edward Schweitzer, TSgt. Paustino Rivera and SrA Joyce Valdivia in warehouse.

Michael Chun, Material Storage & Distribution, James Kawano, Civilian Pay, Brian Tom, Civilian Pay, James Williams Jr., Military Pay; Amn Edward Tang, Motor Pool) from within the squadron who were motivated by a common cause, and who were willing to help each other achieve their goal through a combination of hard work, sacrifice and talent.

Changes of been a constant part of our past and will continue as both part of the present and future. The Sperry 110 system promises to provide more capability to link data from different units and different functional areas such as supply, comptroller, and personnel. With the conversion to the F-15 aircraft and the new challenges it will bring to our operation, the 154th Resource Management Squadron anxiously awaits the opportunities ahead knowing that its fine history is a forecast of outstanding achievements yet to come.



RMS aircraft refueler at work during 154th deployment to Team Spirit '83 exercise in Republic of Korea.



MSgt. Alfredo Canopin and CMSgt. Howard Okita in the Management and systems Branch.



RMS personnel from Van Nuys, California cross-train in the 154th COMPG's RMS facility.



Lt. Col. Robert Y. Nakamatsu
Commander

154th Combat Support Squadron



official emblem

The makings of the 154th Combat Support Squadron CSS) began when the former 199th Support Squadron was federally recognized on Feb. 1, 1960. Col. Walter F. Judd was its first commander. The nucleus of the noted "kokua" outfit was comprised mainly of support-type activities such as personnel, administration, food service and air police.

The proud unit had a short albeit a colorful history when as a result of a major reorganizational change—the birth of the 154th Fighter Group—it was deactivated in November 1960 and its people absorbed by and into the functional areas of the new Group as members of the 154th Air Base Squadron. Nevertheless, critical support services as performed by the "guys" behind the scenes continues unabated—military pay, dining facilities, motor pool/transportation and personnel processing/recordskeeping.

All the loose ends came into place on July 20, 1963 when federal recognition was extended to the 154th CSS—making it a full though separately distinct partner in the 154th Fighter Groups' scheme of things. Lt. Col. Frank W. White assumed command in September of the same year amid a myriad of command and organizational changes.

It was during this initial period that the Consolidated Base Personnel Office (CBPO) was organized to provide the pooling of so many diversified personnel requirements and actions. A year later, a new mechanized personnel data system caused the "phasing out" of the old "morning report"



Lt. Col. Robert Nakamatsu assumes command of 154th Combat Support Squadron from Lt. Col. Richard M. Settsu, who retired in January 1986.

system. Data processing was here to stay.

The years between 1966 and 1976 saw the continued growth of services with the goal of improving the lot of assigned Hawaii Air National Guardsmen. To this end, commanders like Lt. Col. Herbert S. Isonaga and Brig. Gen. Kenji Sumida, Chief of Staff, HQ HANG, (then a field grade officer) paved the way with innovative ideas and strong leadership.

Non-commissioned officer strength lay in dedicated men such as CMSgt. Theodore D. Hussey, MSgt. Obed N. Mansfield and MSgt. George K. Meyer, III. Women too began joining HANG's ranks and Combat Support led the way with the first two women—one officer and one enlisted woman.

The last ten years—1976 through 1986—have seen tumultuous changed in assigned missions and in aircraft possessed—thereby further tasking those functional support areas within the 154th CSS's area of responsibility.

Lt. Col. Richard M. Settsu steered the squadron through these times lending his years of administrative and personnel expertise while relying on old-timers like MSgt. Adam Y. Aki, MSgt. Frank R. Farley, MSgt. Harry Teramoto, and many others.

In January 1985, Lt. Col. Robert Y. Nakamatsu assumed command of the unit now comprised of numerous activities, such as—the CBPO, Food Service, Information Systems, Training (CCTV/Small Arms/Photo Lab), Judge Advocate General, Chaplain, and Social Actions.

A highlight in the 1985 period was this unit's first-ever deployment as a group—a cross-training exercise comprised of forty men and women who swapped places with a like number from the Oregon Air National Guard's 142nd Fighter Interceptor Group at Portland, Oregon.

The success of the operation was due to the planning and coordination efforts of Lt. Col. Colonel Ronald J. Silva, Chief, CBPO and members of his immediate staff.

Though day-to-day support services may be tabbed "routine", it is nevertheless important in the whole scheme of things and the "can do" attitude of the personnel of the 154th CSS who are involved in the numerous support services contribute their share to the mission accomplishment of the Hawaii Air National Guard.

Today's "can do" attitude coupled with yesteryear's spirit of "Kokua" have prepared the men and women of "CSS" to meet tomorrow's challenges associated with the 154th Composite Group's aircraft conversion to the F-15A in its air superiority role.



Combat Support Squadron deployment to Portland, Oregon as part of Support Services Cross Training Program, Feb. 1985.



Lt. Col. Richard M. Settsu, former CSS commander, presents a memento of HANG's visit to Portland ANG Detachment Commander, Col. Jack Dobler.



Chief Master Sergeant Darryl Ho and special assistant SSgt. Iris D. Torres, CBPO, get back to work following production of HANG 40 book.



WHY IS THIS WOMAN SMILING? MSgt. Iris Kabazawa, Base Career Advisor, and Sgt. Vickie Cabilan of CBPO are also happy to see HANG 40 book completed.



TSgt. Don Ho (no, not Mr. Tiny Bubbles) of the Combat Support Squadron's Photo section.

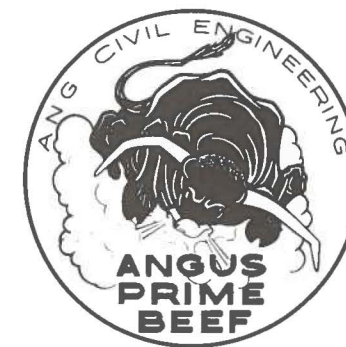


154 CSS personnel "ham" it up for this photo taken in the 154 COMPG Dining Facility.



Maj. Jeffery M. Smith
Commander

154th Civil Engineering Squadron



The 154th Civil Engineering Squadron (CES) has come a long way since its humble beginnings as the Installation Section of the 154th Combat Support Squadron. Headed by 2Lt. Hiram Pang, the section had only 15 members at its inception in September 1967. In October 1969, the Installation Section grew into the 154th Civil Engineering Flight (154 CEF) assigned to the 154th Fighter Group.

During the early years, the talented Civil Engineers were tasked to perform various minor construction and repair projects. Some of the accomplishments included the erection of the Base Chapel at Osan AB, ROK in 1972; construction of the concrete pads and shelters at Anchorage, Alaska for the Alaska ANG in 1973; construction of a concrete helipad at Mt. Laguna, California in 1975; and several mini-deployments to the neighbor islands to complete miscellaneous maintenance and repair projects. The men and women of the 154 CEF also completed several community service projects that included the 1974 relocation of three portable classrooms from Kailua Intermediate School to Blanche Pope Elementary School in Waimanalo.

As the years passed, the reputation and "can-do" attitude of the 154 CEF quickly spread through the Air Force community. The 154 CEF, as a vital part of the 154th Composite Group (154 COMPG), contributed to the outstanding performance of the Hawaii Air National Guard. In September 1977, the 154 CEF deployed to St. Albans, Vermont, where they installed an eight inch fire/water main and a dry chemical fire extinguisher system at an Air Defense Command radar site. They also reroofed ten buildings and completed other minor construction projects. In June 1978, the 154 CEF undertook a major construction project: constructing thirty carports and a motorcycle storage building for the family housing units at Makah AFS, Washington.

For the next two years, the 154 CEF stayed close to home. In 1979, they painted thirteen barracks buildings at Barking Sands NAS, Kauai and reroofed six recreational cottages at Bellows AFS. In 1980, they constructed eighteen bus shelters throughout the island of Oahu for the city and county as part of its beautification project.

July 1981 found the 154 CEF back on the road again, this time to Van Nuys ANGB, California—home of the Hollywood Guard. As before, the team completed various minor construction projects, including working at dizzying heights to install approximately 5000 square feet of drop ceiling in an avionics facility.

During June of the next year, the 154 CEF went "western", and deployed to Cheyenne ANGB, Wyoming, completing approximately \$60,000 worth of construction projects. Some of these projects were the construction of a loading ramp, sidewalk and curb, renovation to buildings and plumbing repairs. In September, the National Guard Bureau increased

the manning of the 154 CEF from 24 total personnel to seventy and tasked the unit with Prime Base Engineer Emergency Force (Prime BEEF) responsibilities.

In February 1983, the stalwart 154 CEF headed back to California, this time to Moffet Field NAS to construct a 6000 square foot War Readiness Material storage facility and renovate the 129 AREFG operations offices.

The unit was awarded for its diligence and professionalism in October 1984 with an increased authorized manning to ninety four personnel and given a WESTPAC Prime BEEF wartime tasking. Two months later, 43 members deployed to Eglin AFB, Florida for the purpose of Prime BEEF Rapid Runway Repair (RRR) training. The Hawaii ANG team trained alongside an Air Force team from 15 ABW/CES, Hickam AFB, Hawaii. This was the first ANG/Air Force joint Rapid Runway Repair bivouac field training. The cooperative spirit of the 154 CEF reflected great credit upon the 154 COMPG and the Air National Guard and received special praise from the camp commandant. After completing the training, the team continued on to McGhee-Tyson ANGB, Tennessee, where they completed several minor construction projects including the refurbishment of the "Hawaii Room" at the NCO dormitory.

In May and June 1985, 35 members of the 154 CEF deployed to Kunsan AB, ROK to complete RRR training. As before, the team (composed of members who had not participated in the Eglin training) trained and worked alongside an Air Force counterpart from the 432 CES, Misawa AB, Japan, and again impressed trainers with their enthusiasm and team spirit. One month after their return to Hawaii, the unit was upgraded to Squadron status. During the remainder of the year, the unit performed home station training.

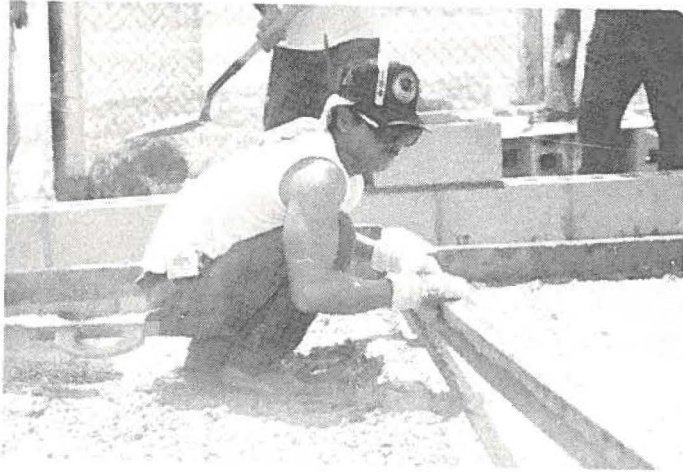
1985 was basically a stay at home and train year, in addition, 31 members completed minor construction projects during two five-day mini-deployments to the 201 Combat Information Systems Squadron (CISS), Hilo, Hawaii and the 201 Combat Information Systems Flight (CISF), Kahului, Maui.

In 1986, the 154 CES proved that you can't hold a good team down, as they traveled to Clark AB, Republic of the Philippines to construct a Flammable Material Storage Building, a 3000 square foot covered Entomology Storage, an addition to the Exterior Electric Shop and helped complete a base perimeter road, all for the 3 CES.

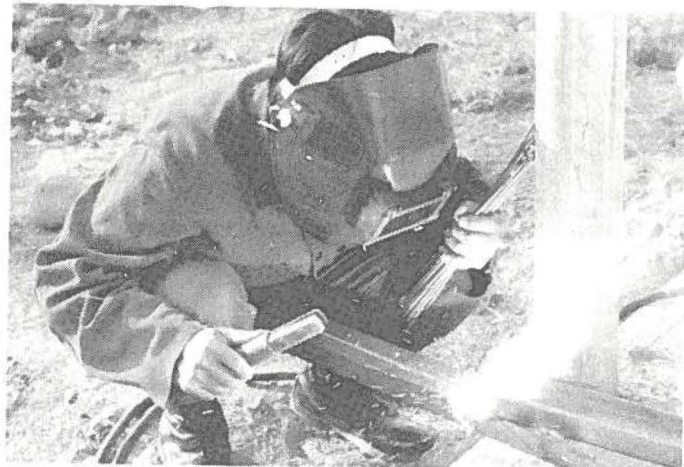
In addition to these key events in 1985, all assigned heavy equipment operators attended (or will attend in 1986) the ANG Red Horse Equipment Operator's Training School at Port Clinton, Ohio and Indiantown Gap, Pennsylvania. Further, the 154 CES has implemented month-by-month and In-Service Work plans to meet wartime and skill upgrade training requirements. Additionally, extra annual training periods have been devoted to bring mobility readiness to

fully qualified status as directed by Air Force Regulation 93-3. During this same period, initial assembly of mobility tool kits and mobility were accomplished and a mobility section was created in the 154 CES storage facility.

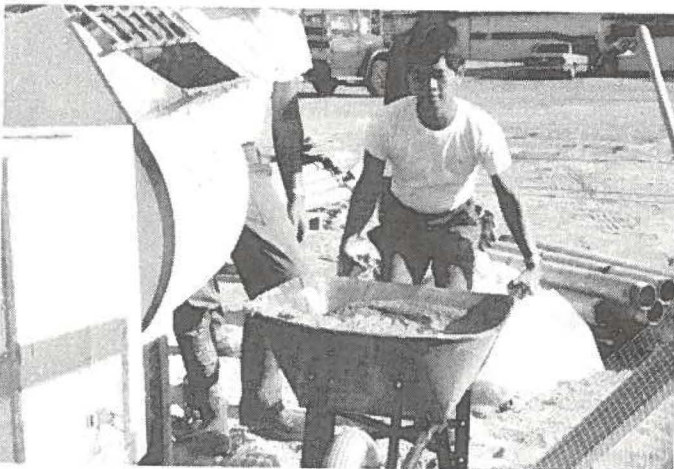
Through these accomplishments, the 154 Civil Engineering Squadron has contributed immeasurably to the total accomplishments of the 154 COMPG. The 154 CES has



TSgt. Bob Davis during P.I. deployment, 1986.



Sgt. Ron Gionson during Philippines deployment, 1986.



SSgt. Jeff Chun (w/wheelbarrow) during P.I. deployment.

stretched and grown from the tiny, fifteen member Installation Section to an outstanding, Prime BEEF squadron which is a valuable PACAF wartime capable unit and is truly representative of the excellence, professionalism and readiness of the Hawaii Air National Guard and the United States Air Force.



SSgt. Tony Kepaa during Philippines deployment, 1986.



SSgt. Chun and SMSgt. Rodney Correa during P.I. deployment.



SSgt. Earl Freitas on deployment with 154th CES this year.



Lt. Col. Norman G.E. Chang
Commander

169th Aircraft Control & Warning Sq.



The 169th Aircraft Control and Warning Squadron (AC&W SQ.) moved into computer age technology when it attained initial operational capability on June 29, 1984 in the Hawaii Regional Operations Control Center (HIROCC), one of eight ROCC's located in the United States and Canada, which make up the Joint Surveillance System. The new building at Wheeler AFB to house the new Air Defense Direction Center (as well as the active Air Force's Air Defense Control Center), the equipment represented an investment of over 20 million dollars. Just as it was when the unit was organized on October 7, 1956 with its headquarters at Sand Island and its operational facility at Koko Crater, its mission has been to detect and identify aircraft entering the airspace surrounding Hawaii, provide intercept information to the fighters scrambled for identification of unknown aircraft, and if necessary, order the destruction of hostile aircraft which threaten our islands. Since moving from Koko Crater to its facilities at Mount Kaala and Wheeler AFB in 1965, the 169th has performed its mission with such skill and dedication as to be rated as one of the outstanding Air Force units.

In Fiscal Year (FY) 1972, the 169th AC&W Squadron was recognized nationally by the Director of the Air National Guard, receiving an "Honorable Mention" award in the field of communications-electronics. Previously, the unit had won the ANG Distinguished Maintenance Unit plaque for placing second in competition among all ANG C-E units. SMSgt. Richard K. Senaha, Non-Commissioned Officer in Charge (NCOIC) of Radar Operations, became the first Chief Master Sergeant of the 169th AC&W Sq. on April 24, 1972. The 169th AC&W Squadron was awarded the National Guard Association of the United States (NGAUS) Air National Guard Communications-Electronics Trophy at the 94th NGAUS Conference in September 1972.

The 169th AC&W Sq. began FY 1974 with a reorganization of the unit's strength with a reduction from 235 to 180 (28 officers and 152 airmen) in authorized strength. The ground power and communications sections were deactivated, but the air police section was reactivated. The 169th was again cited and received an "Honorable Mention" award in the field of communications-electronics by the Air National Guard Awards Board. This marked the fourth consecutive year in which the unit had been recognized for its outstanding work in communications-electronics. The 169th received its initial exposure to the Tactical Air Control System by performing the role of a Control and Reporting Center in its active support of Exercise Cope Elite. Held in conjunction with the U.S. Army's Exercise Opportune Journey, the 169th AC&W Sq. provided radar control of A-7D close air support aircraft from Barbers Point Naval Air Station to the Pohakuloa Training Area on the Island of Hawaii. Weapons controllers additionally underwent initial training in air refueling tactics which

HANG 1986

were employed during a search and rescue exercise and Exercise Cope Elite.

The 169th AC&W Sq. was again selected for the Air National Guard's Outstanding Communications-Electronics and Meteorology unit for FY 1975. Further, the unit was presented with the Air Force Outstanding Unit Award (AFOUA) for exceptionally meritorious service in support of military operations for the period July 1, 1973 to June 30, 1975, with special recognition of the Pacific Air Forces (PACAF) Weapons Controller Requalification Course operated by the unit, the first HANG unit to receive this prestigious award. Lt. Col. Paul Goya, unit commander, transferred to the 201st Mobile Communications Group (MCG), and was replaced by Lt. Col. Donald M. O'Day. The 326th Air Division's Air Defense Control Center was co-located with the unit's Air Defense Direction Center in building 204 at Wheeler AFB. The unit received the National Safety Council's Certificate of Commendation based on a perfect ground safety performance in 1976.

The unit performed extremely efficiently during the PACAF Operational Readiness Inspection. The effort of all unit members earned the unit the HANG Outstanding Unit Award for fiscal year 1977. The unit further received the National Safety Council's Award of Commendation for the second year in a row for noteworthy ground safety performance during 1977.

The 326AD Stan/Eval team conducted a formal visit to the 160th AC&W Sq. for which the unit received an excellent rating. Capt. Melvin Sumida and MSgt. Edwin Doi participated in HANG's first deployment to Combat Sage in the Philippines. With a deployment to Guam as well, a total of 14 unit members participated in deployment activities during the year. On Jan. 25, 1979, the Department of Defense, State of Hawaii General Order Number 4-HI was issued announcing the realignment and reorganization of the AC&W units and assigned the 169th to the 154th Composite Group at Hickam AFB. The unit received the ANG Distinguished CEM Unit Plaque at the 101st General Conference of the National Guard Association of the United States.

The 169th celebrated its 25th anniversary with a party on Oct. 3, 1981, at the Skyview Terrace at Fort Shafter. Fun was had by all as we were treated to a slide presentation reflecting on our first 25 years as part of the HANG.

The 169th AC&W Squadron won the Outstanding Tactical Control Unit award for unit achievements from Jan. 1, 1982 through Dec. 31, 1982. Additionally, the unit was presented the National Safety Council Award of Commendation.

Fiscal year 1983 was highlighted by the dedication of a new building housing the HIROCC, part of the nationwide Joint Surveillance System, on Sep. 24, 1983. This new home for the 169th was designed to upgrade the Hawaii Air Defense

System by utilizing state-of-the-art technology and equipment. Its computer processes large amounts of data and displays the information in near-real time, replacing the radar scopes and manual plotting boards used by the HANG since 1956. Among the dignitaries present were Governor George Ariyoshi, U.S. Representative Daniel Akaka, Adjutant General, Maj. Gen. Alexis Lum and Commander, HANG, Brig. Gen. Edward V. Richardson. With the new HIROCC computerized system and over 25 years of manual operations, 1983 meant new schools and intense in-house training for the members of the unit. With many members attending schools in Keesler and Tyndall AFB, the 169th was able to maintain its 24-hour Air Defense and make a smooth transition into the computerized system.

Members from radar operations participated again in Team Spirit 1983 in the Republic of Korea; the participants were assigned to detachment units of the 621st Tactical Control Squadron at Yongmunsan Manilsan as well as at Osan AB.

For the second time, the 154 COMPG received the prestigious AFOUA for the period Jan. 1, 1982 through Dec. 31, 1983; as part of the 154 COMPG, the 169th was cited for its participation with the Third Fleet Training Group, Marine Aircraft Group, and helping FAA commercial air traffic to transmit safely. This was a significant step toward true joint use of our Hawaii Air space in a manner which can reasonably accommodate FAA transit without adversely affecting the military activity therein.

The year 1984, was highlighted by the Initial Operational Capability of the HIROCC, a culmination of the installation and checkout of new computerized, state-of-the-art equipment and the retraining of squadron personnel to operate and maintain the new system. The first ACT/DACT mission was conducted on Mar. 20, 1984. On May 22, 1984, operations



Hawaii Governor George R. Ariyoshi (center) heads the distinguished delegation present for the groundbreaking of the HIROCC building.



SSgt. Sam Achuela in the HIROCC Computer Room.

at the HIROCC were initiated on a 12 hour per day basis and on June 10, operations assumed full 24 hour manning of the HIROCC. A live System Air Defense Exercise was conducted on June 23, 1984. The System Turnover Certificate for the HIROCC was dated June 29, 1984, meaning we were finally fully operational.

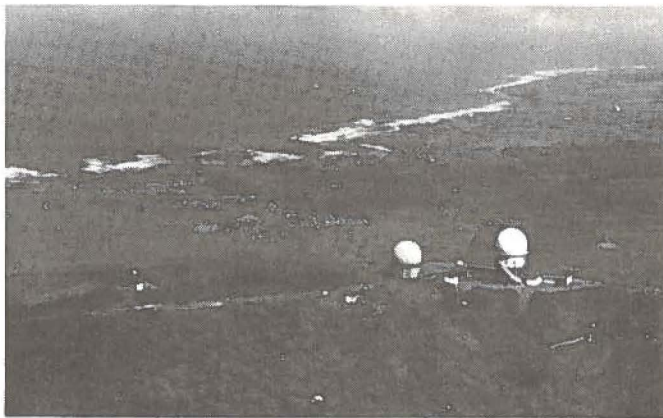
The Director of the Air National Guard, Maj. Gen. John B. Conaway, announced the selection of the 169th AC&W Squadron to receive a NGAUS Mission Support Award for the units performance during the period June 1, 1984 through Dec. 31, 1984. Of particular significance was the attainment of Initial Operational Capability of the HIROCC ahead of schedule and successfully passing the PACAF IG MEI/ORI within five months thereafter.

Maj. Arthur Kimura, drill status weapons controller and full time school teacher was selected as one of two teachers from Hawaii to participate in the NASA Teacher in Space Project; as a finalist from the initial 10,000 applicants. He was one of 114 teachers who convened in Washington D.C. where the final selection of the teacher to fly on the space shuttle took place.

The new year started with a change of the unit commander. Lt. Col. Donald M. O'Day, Commander, retired on Jan. 3, 1986. Taking over as unit commander was Lt. Col. Norman Chang. He joined the HANG in October 1957 as a Weapons Controller and a transport pilot flying the C-47 and C-54. Prior to becoming the unit commander, he was a Flight OIC, Senior Controller and Senior Director as a member of the 169th AC&W Sq. More changes to the 169th occurred as Lt. Colonel Paul Sequeira, Weapons Director Staff Officer, transferred to the 154 COMPG at Hickam AFB and Lt. Col. Steve Oka took over as the new Director of Operations for the 169th.



Sgt. Donna Padello at Operation Display Console at the Hawaii Regional Operational Control Center (HIROCC).



The "Eyes and Ears" of the 169th Aircraft Control and Warning Squadron, Mt. Kaala.



Lt. Col. Owen D. Ota
Commander

150th Aircraft Control & Warning Squadron



official emblem

KAUAI O MA NO KALANI! Translated means "GUARDIANS OF THE HEAVEN," an appropriate motto for the 150th Aircraft Control and Warning Squadron (AC&W Sq.) tasked to guard the westernmost reaches of the State of Hawaii against intruders.

Surrounded by the beauty of the Garden Isle, the remote radar site sits nestled 4,225 feet on Mount Kokee within sight of the "Grand Canyon of the Pacific" to the south; the rainbow hued Kalalau Valley to the north; and the "Wettest spot on earth", Mount Waialeale to the east.

Twenty-six years ago (1961), a detachment of 12 officers and 77 Airmen from the 109th AC&W Squadron at Kahuku Point on Oahu left the Punamano Station for Mount Kokee on the Garden Isle. Members of the Oahu based unit were met at the Lihue Airport by Capt. John English, SSgt. Mirvine Hookano, A1C Kiyu Inouye and A1C Kiichi Fujii. Debarking from the C-47 came the nucleus of young troops which included: Capts. Don O'Day and Danny Au, SSgts. Francis Hosaka and Kenichi Sonan; and Amn. Bill Crowell.

At exactly 8:00 a.m. hours on Mar. 15, 1961, the antenna of the FPS-20 at Mount Kokee began rotating to assume the 24-hour round-the-clock air defense operations for the State of Hawaii.

This was both a beginning and the end of an era. This was the end of the era of the F-86L, Super Saber, which was the main weapons system for the HANG. 90 beam rocket attacks were the primary tactics positioned in three ship formations by weapons controllers using their handy-dandies on grease-penciled covered UPS-35s. Operational Readiness Inspections (ORIs) were 72 hours round-the-clock affairs and live rockets were fired against towed targets during ORI controlled phased exercises. The Khaki 505s were in vogue.

Capt. Dave Dang and TSgt. George Koga insured that the FPS-20/FPS-6 were turning while SSgt. Richard Farm kept us talking on the GRC 7s/GRC 3s.

The year 1961 ushered in major changes for both the HANG and the 150th ACWS. The missile firing, supersonic, sweptwinged F-102 "Delta Dagger" was introduced into the HANG replacing the reliable but ancient F-86L. The 109th AC&W Sq., Detachment 1, was deactivated and replaced by the new federally recognized 150th AC&W Sq. on Oct. 21, 1961. Maj. William Mossman was appointed the Commander of the newly established squadron.

Thus the 150th AC&WS entered a new era characterized by the front stern reattack tactics with the missile capable F-102s. Weapons controllers were awarded qualified, skilled, and expert ratings. Barking Sands was called Bonham ALF (Auxiliary Landing Field) named after an ex-HANG pilot. Young Lts. Clarence Jim and Walter Watanabe served under the more experienced Capts. Waichi Kikuta and Steve Millard (an ex-326AD Chief Controller).

The local communities quickly adopted the "boys" of the

150th AC&W Sq. as their own and in appreciation for their hospitality and Aloha, the unit hosted an Appreciation Luau inviting over 500 community leaders, supporters, and families at the Kekaha Armory. It proved to be one of the major events of the year as reported by the Garden Island Newspaper. The 150th continued to interact with the local communities by hosting an open house at Kokee where 800 visitors toured the Kokee facilities. The squadron's official greeter was Miss Cheryl Eguchi, a Waimea High School senior who was crowned Miss Stargazer for the occasion. The wives got into the act by forming their own club with Emmaline English serving as President. Her cabinet included: Barbara Hosaka as Vice President; Alice Nishihara as Secretary and Grace Takabayashi as Treasurer. The Club's first project was the beautification of the Hanapepe river bank with the men of the 150th planting over 50 plumeria trees. Another eye opener was the flood of silver dollars circulated within the communities when all unit members were paid in silver dollars.

A major change in command occurred in 1963: Maj. Bud Bye assumed command from Maj. William Mossman; Capt. Raymond Moriguchi replaced Capt. Donald O'Day as Ops Officer; and Capt. Allen Mizumoto assumed the C/E post from Capt. Dave Dang.

The unit continued to prosper as highlighted by major events such as the "TRY ONE" recruiting drive. To publicize the drive, the unit sponsored a 50-mile walkathon from Mana to Anahola. Twenty-five started the walk; one finished. SSgt. Wayne Soma completed the hike in 16 hours and 50 minutes. The drive netted three new weapons controllers for the unit: 2Lts. Wayne Ellis, Tommy Cox, and Ron Tanaka. This was also the end of two weeks "live-in" summer encampments with personnel living at the CCC Camp (now the Kokee Ranger Station) roughing it in 45° weather. The highlight of one of these camps was the exchange program between the 150th AC&W Sq. and the 130th AC&W Sq. from Salt Lake City, Utah. Twenty hardy Mormons from Salt Lake City exchanged positions with 20 fun-loving Hawaiians from the 150th. The Mormons taught the Hawaiians how to survive in 40° weather by wearing long johns and the Hawaiians taught the Utah unit how beer should be consumed. It was reported that the Hawaiians consumed in one night what the Mormons consumed in one year. Unit members were also recognized through the HOKU AWARDS program: MSgt. Emilio Olivas was the first recipient of the HOKU ALII AWARD as the Outstanding Airman of the Year; TSgt. George Susterich was recipient of the HOKU AWARD as the Outstanding Airman for the quarter; and A3C Timmy Albao the recipient of the HOKU HOU AWARD as Outstanding First Termer of the Year.

The emphasis shifted in the seventies from an air defense to an air superiority role for the HANG. In 1976, the HANG

received the dual roled F-4C "Phantom" jets, work horse of the Vietnam war, replacing the pure F-102 air defense fighter. The 150th also underwent a command change with Maj. Raymond Moriguchi taking over command from Lt. Col. Bud Bye. Maj. Owen Ota assumed the Operations Officer's position. The 150th AC&W Sq. also became a unit within the 154 Composite Group (COMPG) under the command of Col. John S.W. Lee.

With the assumption of the air superiority mission by the 154th COMPG, a whole new arena of deployments opened up for the 150th AC&W Sq. In 1978, the 150th participated in its first deployment to Cope Thunder/Combat Sage in the Philippines. Deployments were the order of the day as deployments followed in 1979 to Cope Road (Guam) and Cope Thunder (Philippines); in 1980 to Cope Road (Guam) and Cope North (Misawa, Japan), the first time that an Air Guard unit participated in a joint exercise with a foreign nation. In addition to the deployments, the 150th was tasked with PACAF Inspector General (IG) augmentation to the Philippines, Korea, Okinawa and Alaska. Weapons controllers were also tasked as augmentees to Team Spirit and Focus Lens exercises in Korea.

In 1979, Maj. Reggie Doi and Norman Chang were the first HANG controllers to become ACT/DACT qualified by Aggressor Controllers at Wallace AS in the Philippines. The era of the air defense intercept gave way to the tactical intercept to engagement and terms such as "furbal", "knock it off" and "merge" became the standard language of controllers and aircrews. Apprenticed controllers Capt. Gary Nitta and Lts. Myles Moriguchi and Glen Kagawa joined the ranks of the 150th AC&W Sq.

The unit shared the honor of Air Force Outstanding Unit Award (AFOUA) awarded to the 154 COMPG for 1978-79.

The 150th AC&W Sq. continued to foster excellent relations with the local communities through such projects as the Senior Citizens' Field Day, Super Kids, and Color Guard support of community events. MSgt. Lester Matsushima enhanced unit and community pride and morale with the All-HANG Ahi Tournament in 1982, which has generated com-



150th members man the watch over Hawaii's skies.



Maintenance and communications play an important role in the 150 AC&W Squadron's mission.

munity-wide recognition for the unit.

As the 150th AC&W Sq. transitioned into the 80s, two major events impacted heavily on the unit. HURRICANE IWA struck Kauai and Kokee with destructive force and impact. Kokee AFS remained on the air throughout the storm tracking the eye of the hurricane as it smashed through the island. Although withstanding winds of over 100 knots, and buildings buckled under the hurricane force winds, Bravo crew under Maj. Myron Dobashi's supervision rode out the storm providing valuable information to the system and the weather bureau. Through the efforts of unit personnel, the station was operational within a week. Two weeks after the Hurricane, the PACAF IG team descended on the unit for an ORI. The 150th requested to be evaluated and came away with an "OUTSTANDING" rating.

In 1983, Lt. Col. Owen Ota took command of the unit from Lt. Col. Raymond Moriguchi who was transferred to HQ HANG. Lt. Col. Reginald Doi became the unit's Operations Officer.

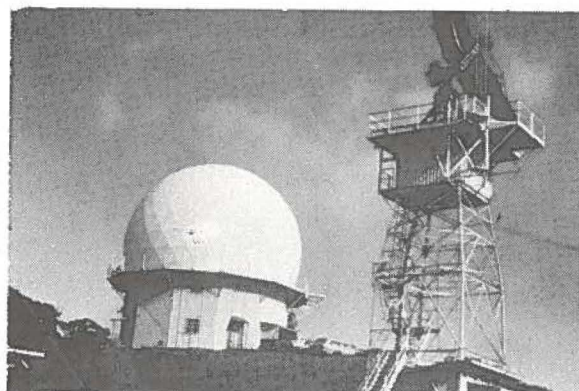
In 1984, the HANG took a giant step forward in the AC&W ground environment arena when it acquired a REGIONAL OPERATIONAL CONTROL CENTER (HIROCC) at Wheeler AFB. Computers replaced the scope operator/plotter in providing surveillance information for the HADS. The 150th AC&W Sq. went through a reduction-in-force action losing 23 positions to the HIROCC. Original members of the unit MSgt. Bill Tabe and MSgt. Hank Sato joined the ranks of the retirees. Although air defense activities focus at the HIROCC, Kokee AFS and the 150th continued to play a vital role in the air defense of Hawaii with its time tested UPA-62s and experienced and seasoned AC&W operators and controllers.

As we celebrate the 40th Anniversary of the Hawaii Air National Guard, the 150th AC&W Sq. stands proud as one of its outstanding units making a positive contribution to the success of the mission of the HANG. We can take pride in our continuing efforts towards providing for the air defense of Hawaii as the "GUARDIANS OF THE HEAVEN".

KAUAI O MA NO KALANI!



Inside the radar dome at Kokee AFS, Kauai.



The 150th AC&W Sq.



Lt. Col. Norberto Baysa
Commander

154th Tactical Hospital



Join us as we step aboard the wings of our caduceus, the herald staff symbolic of the medical service corp, and are whisked through time to the early 1950s. Our final approach and decent brings us to an old World War II, 12x12, wooden hut that serves as a first-aid station for HANG personnel. The staff of the first aid station is a physician, Dr. Claude V. Caver (Remember Dr. Caver? He was the redheaded dermatologist with the swagger stick and elegant cigarette holder held in a jaunty manner ala Roosevelt) and two enlisted men with no medical experience. These were the beginnings of the HANG's 154th Tactical Hospital.

Time continues to advance forward as we approach May 1957, the time when the HANG supplements its medical element with its first full-time Health Services Technician. Sgt. Toshiyuki Watabayashi, "Toshi", as everyone calls him, spent many long arduous hours scrubbing and cleaning that 12x12 wooden hut that he finally refurbished into a more decent first aid station.

The first unit manning document of this unit included one flight surgeon, one aerospace medical technician and one laboratory technician. With Dr. Caver and Toshi on board, Leslie I. Nakashima was recruited from the Weather Flight to fill the position of lab technician. In December 1960 the unit was formally recognized as the 154th Dispensary and the manning document was increased to two flight surgeons, and six enlisted personnel.

Things began to look brighter for the medics and they started moving up. In 1967 with the dedication of Building 3400, the dispensary relocated to the back half of the third floor sharing space with the USAF advisors and the Combat Support Squadron. As workload increased the dispensary was authorized a second full-time Health Services Technician and in April 1969 filled the position with Mitchell K. Shimabukuro. Also during this time period Dr. Caver became the State Air Surgeon. Dr. Cas Jasinski succeeded him as dispensary commander. Cas was a navy pilot originally, and then went to medical school.

As the mission of the HANG changed and its units grew in size, so did the dispensary. Since 1960 when it was first recognized as a numbered dispensary, it has had two subsequent name changes, first in 1973 as the 154th USAF Clinic and more recently as the 154th Tactical Hospital. It has also grown from that initial UMD of two flight surgeons and six enlisted personnel to 14 officers and 45 enlisted with two off-location, geographically separated medical units. Speaking of geographically separated units... our first off-island summer camp was to Kokee, Kauai, in 1978. Memorable events then were the cold, wet mornings and Bob, or was it Stan, getting the bus and a full load of medics stuck in a ditch half way up Mt. Kokee. Luckily, a kindly fellow in a 4-wheel jeep stopped and hauled the bus back on the road.

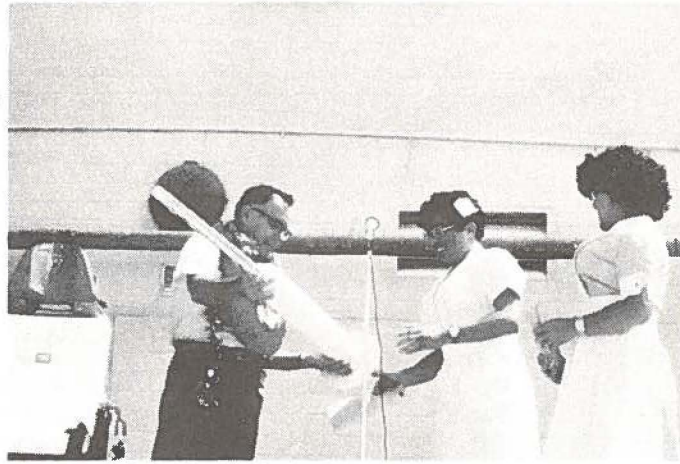
The clinic's mission changed dramatically in 1979, with the addition of mobility and medical readiness training require-

ments. Dr. Walter Chang assumed command during this period of dynamic growth and change. In its recent Health Services Management Inspection (HSMI), the hospital graded an outstanding in its mobility and medical readiness training. The designation of its new mission capabilities has enabled the hospital staff to venture beyond the Hawaiian Islands to perform its annual training. In 1981 hospital personnel travelled east and deployed to the David Grant Medical Center, Travis AFB, California. The years to follow took us to WESTPAC and began our international deployment. Our first stop was at the Regional Medical Center Clark, Clark AFB, Republic of Philippines. The following year lured us to the "Land of the Rising Sun" to perform duty at the USAF Hospital Yokota, Yokota AB, Japan. The subsequent two years rotated our deployments between Clark and Yokota AB. This year's deployment took the hospital personnel to the "Land of the Never Setting Sun" as training is performed at the USAF Regional Hospital Elmendorf, Elmendorf AFB, Alaska.

Although training is the hospital's primary mission, providing medical support to all the HANG units is the focal point of our peacetime activities. Keeping the 154th Composite Group and the 201st Combat Communications Group combat ready for world wide duty is an assignment that is never ending. MASH trips (mini-deployments to the neighbor islands) to provide medical support to our neighbor island units are done monthly. The coordination of these mini-deployments which involve ground and air transportation, supplies and equipment, and personnel is carried out behind the scenes by MSgt. Mitchell K. Shimabukuro. Without his efforts the hospital would not be able to mount the wings of its caduceus.

Mission readiness training and operational effectiveness is the military side of the Tactical Hospital, but lending a helping hand to the community we call home has been an integral part of our activities. Back in 1962 personnel from the then dispensary assisted in the administration of the oral polio vaccine. In 1972 they also participated in the administration of flu vaccines and TB skin test to the patients of Waimano Home. Participation in the Statewide Civil Defense Disaster exercises has been an ongoing, recurring activity that the hospital supports. The hospital also supports events like the Honolulu Marathon, Pepsi 10-K run and the Women's 10-K run with a team of volunteers to man an aid station. This is only part of the role that the Tactical Hospital plays in being a leader and supporter of community activities.

As we continue on the final leg of our venture on the wings of our caduceus, we cannot help but gaze into the future and wonder what will be the role of the Tactical Hospital? Only time and the world situation will tell, but until we can focus more clearly on what the future holds, we the members of the 154th Tactical Hospital will continue to be mission ready and support the HANG for another 40 years.



NOW THIS WON'T HURT A BIT. 154th personnel receive immunizations as required to keep them ready for mobilization. And if this one doesn't mobilize you... nothing will!



Maj. Warren Shibuya (CSS) "grins and bears it" as he gets a shot from MSgt. Toshiyuki Watabayashi, now retired.



Keepin' HANG healthy.



"Service with a Smile!"



Preventative health care. 154 personnel receive annual flu immunizations.



154th Tactical Hospital personnel administer aid to this "victim" during a training exercise.



Maj. Gervin K. Miyamoto
Commander

154th Security Police Flight



On Jan. 5, 1979, the National Guard Bureau (NGB) announced federal recognition of the 154th Security Police Flight (SPF). Prior to this date the Security Police were assigned to the 154th Combat Support Squadron (CSS). Since becoming officially recognized, the unit has strived to become proficient in the security police field and support the "Total Force concept of the United States Air Force."

Lt. Clifford Fukuda commanded the flight from January 1979 to November of that year assisted by NCOIC, MSgt. Albert Keliinui. Maj. Warren Chung assumed command in December '79 through April 1982 with NCOIC, MSgt. Harrison Ho. Today Maj. Gervin Miyamoto commands the flight. SMSgt. Harrison Ho is still the top enlisted member. Throughout these years Leonora Lee and SMSgt. Ho have provided the technician force stability for the flight.

The unit is authorized one officer and 35 enlisted personnel. There are currently one officer and 38 personnel assigned.

Besides receiving monthly training in their perspective tasks as security police personnel, members of the 154 SPF have been deployed for active duty to other bases in order to upgrade and refresh their proficiency.

In 1979, TSgt. William Anana and SSgt. Elmer Tady deployed to the Cope Thunder Exercise at Clark Air Force Base in the Philippines. They integrated with security police force personnel stationed at Clark. After receiving an orientation of the base and operations, they became active participants in maintaining vigilance over the resources on base. The experience provided not only invaluable experience and knowledge relating to a specific career field, according to SSgt. Tady, but also gave much insight into the culture of the people.

Members of the 154 SPF deployed in 1980 with other units of the 154 Composite Group to Misawa, Japan for Cope North a joint exercise between the Guard units and the Japanese Air Force. The unit joined with security personnel stationed there to protect resources during the night hours when the operations ceased. During time off, they got to visit the surrounding areas and learn about the Japanese culture.

The entire flight deployed to Lackland AFB in San Antonio, Texas in 1982, completing 72.5 hours of the Security Police Tactics Course. The course included running the obstacle course which required members to scale walls, low crawl under wire fencing and physical conditioning. Classroom study, daily inspections and practical field exercises in tactics of combat counter insurgency helped build a strong comradery and esprit de corps among the men. During non-duty hours, unit members traveled into San Antonio and Mexico. We traveled to and from Lackland aboard a C-130

HANG 1986

aircraft, a most unforgettable experience.

During the firing exercise at Kaneohe Marine Corps Air Station in 1984, the entire unit was deployed to provide daily security on F-4 aircraft. This two week operation afforded members a chance to refresh their training in providing and maintaining a restricted area around parked aircraft on a 24-hour basis.

In February 1985, it was off to Portland, Oregon where 154th SPF members were hosted by the 142nd Fighter Interceptor Group. Our people were introduced to a security system which involved closed circuit television surveillance on resources. They were also given an opportunity to see the host security personnel in action in their capacity as law enforcement and security personnel.

In August of that year, 154th SPF members kept the vigil for two weeks over parked F-4 aircraft involved in firing exercises at Barking Sands Naval Air Station on Kauai. Because there were no permanent housing facilities on the base, units of the 154 Composite Group were quartered in tents for the duration of the deployment. The security police along with mess personnel were quartered in a storage building because of their unusual hours. Those who were there will no doubt remember: sleeping on army cots draped with mosquito netting; using the "community lua"; listening to your snoring neighbor; and, lastly, the dust. The area of the camp site was so dusty that certain areas had to be watered down to alleviate these terrible conditions. Despite all the inconveniences, it was quite an experience for all the participating individuals. Besides reinforcing their previous training, our people learned to weather adverse conditions and still carry out their mission.

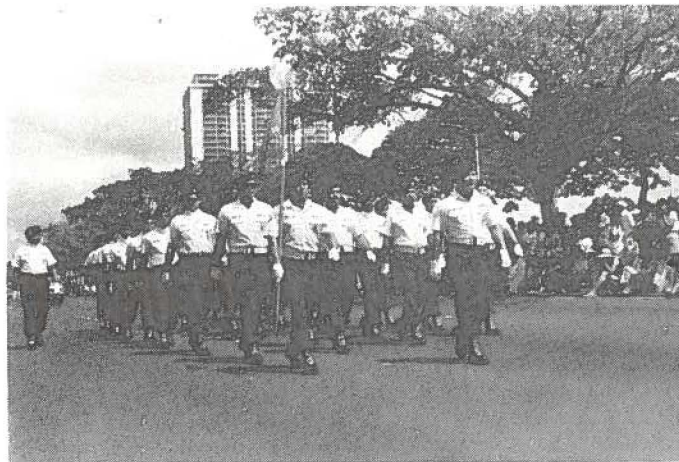
As with other HANG units, the 154th SPF has participated in various community service projects. For example, the unit along with members of the 169 Security Police Section were selected as the "best Marching Unit" in the "Queen's Jubilee Parade" on the 4th of July, 1983.

154th SPF members donated their services in traffic control during a fund raiser for terminally ill children. The unit also assists in the annual Hawaii National Guard Christmas Concert that benefits Guard members and their families.

And the unit has initiated a Child Identification Program for the Hawaii Air National Guard by fingerprinting children. The program was initiated during the 154 Composite Group Family Day activity in April 1986. This program and the deployments described previously demonstrate the kind of dedication and initiative that has brought the 154 Security Police Flight to where it is today and that will carry it into the future as a vital component of the Hawaii Air National Guard.



PRESENT COLORS!



DRESSIN' IT UP. The 154th Security Police Flight participating in the annual Kamehameha Day Parade.



READY, AIM. . .154th SPF personnel maintains proficiency in all aspects of their job.



154 Security Police Flight during State to State Crosstraining deployment to Portland, Oregon.



Lt. Col. Christopher W.K. Jay
Commander

199th Weather Flight



The 199th Weather Flight (WF) is one of the Hawaii Air National Guard's (HANG) charter units. Founded in September 1946, its original mission was to provide weather forecasting and observing services to the pilots of the HANG. This mission has changed since moving to Wheeler AFB in 1982. The 199th WF now provides meteorological services to all aviation elements utilizing Wheeler AFB.

Like many of the Air National Guard's units, the 199th WF has benefited from the adoption of the Air Force's Total Force policy. This policy states that ANG units will be equipped and trained to meet a wartime commitment and be prepared to deploy anywhere in the world. To the 199th WF, this policy meant moving into the base weather stations to work directly with the active duty forces during UTAs.

In the early 1970's the unit started operating at the 154th Fighter Interceptor Group in what was designated a Representative Observation Site (ROS), using a mobile meteorological van. The ROS observer relayed surface observations to the HANG Weather Station by means of a field telephone. The ROS function fulfilled an Air Weather Service requirement for a continuous weather watch during all flying activities. Additionally, a Pilot Balloon (PIBAL) program had also been established to measure the winds aloft in support of flying operations. The two programs gave the 199th WF the capability of providing weather support to any flying unit at any base.

Presently, the flight is fully manned at 13 members and consists primarily of Weather Forecasters and Observers. The observers make weather observations which consist of discerning cloud types and heights, wind direction and speed, temperature, dewpoint, and barometric pressure. The forecasters in turn use the information from the observations as well as Weather Facsimile maps and other data to construct weather forecasts.

During the past few years, the flight's ability to produce more accurate forecasts has been enhanced by the addition of the Harris Laser Satellite Facsimile Machine. Sensor equipped weather satellites are rapidly becoming one of the most important data gathering mechanisms in meteorological equipment inventory. These satellites provide cloud cover data for "data sparse" areas, filling in the observational void that has plagued meteorologists for decades. Another recent addition to the Wheeler Base Weather Station is a computerized Local Weather Dissemination System which provides fast, accurate weather information to customers utilizing base weather services.

Throughout the years the Weather Flight has had its cast of characters. Former commander, Guernsey Curran III, gained fame and wealth by inheriting part of the Singer fortune. Former weather officer, Roy Matsuda, continued his career as a high-salaried forecaster with the National Weather Service. Ex-observers, amiable Lee Cashman, became athletic director at Kalaheo High School while Clyde Yamada,

initially pursuing a promising future in engineering, now operates his own real estate firm. Another member, Les Matsuura, once an administrative specialist, rose to the rank of Assistant Director/Educational Coordinator of the Waikiki Aquarium from humble beginnings. Many other individuals with ties to the Weather Flight have made their marks in the community in such endeavors as accounting, law, sales, and government to name a few.

In the past ten years the 199th WF has had numerous personnel changes. The 199th WF is the smallest of the HANG units with half of the flight occupying the weather station at any one time while the other half is performing drill in the training office. The 199th WF has had a high percentage of BMTS and technical school Honor Graduates and a college-educated enlisted force. Currently, of the 13 flight members, six have college degrees while five others are enrolled in college degree programs. As one can see, education plays an important role in the 199th WF.

During Fiscal Year (FY) 1980, SrA Antonio Querubin, representing the 199th Weather Flight, won the HANG Outstanding Airman of the Year Award.

In 1981 the 199th WF was tasked with an Army support mission. To Weather Flight members, this tasking meant providing weather support while camping-out in a tactical environment, eating C-rations, erecting tents and antennas, practicing camouflaging techniques and in general playing "Army".

Fiscal Year 1982 was a memorable year for the 199th WF. The flight moved into its new home at the Wheeler Base Weather Station after many years at Hickam. Its members participated in the HANG's Half Marathon and the HANG Tennis Tournament. To cap off the year, the 199th WF received the HANG Outstanding Unit Award.

During 1983 the Weather Flight moved into a new training office in the Hawaii Army National Guard Armory on Wheeler AFB. This location has been deemed the Weather Flight's permanent home.

In 1984 the 199th WF received the National Safety Council President's Award letter for excellence in providing a hazard-free work environment and safety education to its members, which resulted in an accident-free year. Also during this year, the flight underwent a conversion that resulted in the replacement of its active duty Military Airlift Command (MAC) Technical Advisor with a full-time Active Guard Management Assistant. This was the first time in the unit's history that all functions performed by the unit were being handled by Air National Guard (ANG) personnel.

FY 1985 saw the 199th WF undergo numerous personnel changes. It acquired two highly qualified prior-service observers via the Air Force's Palace Chase Program. Sgt. Rodney Jacques joined the unit from Detachment 7, 1st Weather Wing, Wheeler AFB and Sgt. Myles Nonaka from the 54th Weather Reconnaissance Squadron, Anderson AFB, Guam. Sgt. Jacques received training in Rocketsonde and

Rawinsonde upper-air sampling techniques and is currently employed as a Meteorological Technician with the National Weather Service in Honolulu. Sgt. Nonaka was trained in the exciting field of Weather Reconnaissance and spent much of his time in the Air Force chasing hurricanes in a C-130. He was also a Basic Military Training School (BMTS) Honor Graduate and received the Air Force Humanitarian Service Award in connection with the Mt. St. Helens disaster of 1982.



SSgt. Jean Burrell checks wind velocity & direction with the anemometer.

Other recent additions to the flight include SrA Colin Beal, a BMTS Honor Graduate, A1Cs Audrey Hilario and Pedro Velasco.

With its mission goals ever before them, the men and women of the 199th WF are constantly striving to provide the best weather services available to aircrews and all other Armed Forces personnel in need of their unique skills.



A1C Pedro Velasco prepares to launch weather balloon.



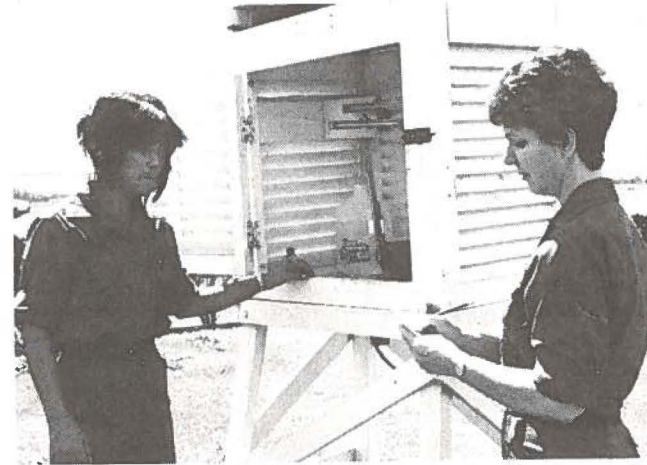
TSgt. Peter Spurlin posts computer generated Weather Analysis and Prognosis charts.



Sgt. Miles Nonaka inflates weather balloon.



TSgt. Tony Querubin posts satellite photos for interpretation.



SSgt. Jean Burrell and Amn Audrey Hilario monitors the equipment shelter for temperature verification.

201st Combat Communications Group



Col. Ernest T. Shima
Commander



official emblem

The 201st Combat Communications Group (CCGP) has a short but interesting history. The Group Headquarters, as it is known today, was established in 1975, but its true beginning dates back to early 1967.

On March 6, 1967 the 109th Aircraft Control and Warning Sq. formerly located at Kahuku was deactivated putting an end to the soft life associated with fixed facilities. That same day, the 201st Mobile Communications Squadron (MCS), a communications unit on wheels, was born. Maj. Paul S. Mori assumed command of this new unit with an original complement of approximately 100 officers and airmen from the deactivated squadron. Rapidly, before anyone could say no, they quartered themselves in interim facilities at Battery Selfridge, a World War II gun emplacement located at Hickam AFB.

In October, the newly formed unit was reorganized with the establishment of four Flights under the Squadron: the 201st and 202nd Mobile Communications Flights, and the 201st and 202nd Flight Facilities Flights. With no place else to go and a lack of prime equipment, the five units remained at Battery Selfridge and operated as a single unit with an operation patterned after the 1st Mobile Communications Group at Clark Air Base in the Philippines.

The early days were rough. Everyone up and down the chain had to learn the new mission, new equipment and most of all, the new way of life. Tents became a home away from home, two and a half ton M-series trucks a means of transportation, and noisy generators the only source of electric power. No mention will be made of latrines or showers. Mobility became the name of the game, and Dillingham Airfield a favorite camp ground.

In just two years of existence, the units had gained sufficient experience and know-how to support a flying unit under field conditions. During annual field training, personnel of the 201st set up and maintained inter- and intra-site communications at forward operating bases at Dillingham Airfield on Oahu and Pacific Missile Range Facility at Barking Sands, Kauai, in support of the 154th Fighter Group operations. Our mission, to provide command and control communications and air traffic control services for the United States Air Force, came into focus. The 201st MCS was on the move.

The squadron became unique among the Air National Guard mobile communications units. It was asked to undertake many support missions for the active Force. These support missions included "Leprachaun Laughter," which provided communications for the Military Airlift Command; and "Sea Spirit," a Southeast Asia Treaty Organization (SEATO) exercise in Thailand. The squadron also deployed personnel to Pago Pago in American Samoa to provide communi-

cations in support of the recovery force for the Apollo missions, and a mobile low frequency radio beacon to Johnson Island.

In addition, the Squadron also took part in developing two "Commando Omega" satellite communications ground terminals for the Pacific Communications Area (PCA). Work on this project was at the request of the PCA commander, Brig. Gen. Harold R. Johnson, and was completed in conjunction with the 1st MCG and other Air Force Communications Service personnel.

Finally, in September 1970, the squadron abandoned Battery Selfridge and moved into a new million dollar facility at Hickam AFB. The new facility provided the needed floor space for all the unit functions and is presently occupied by the Group Headquarters and the 202nd Combat Communications Flight.

The 201st MCS was again reorganized on Dec. 10, 1975 when it was federally reorganized as the 201st Combat Communications Group. Lt. Col. Paul Goya, previously assigned as commander of the 169th AC&W SQ, assumed command of the newly formed Group. Since its inception, the Group Headquarters has provided policy, guidance, and assistance to subordinate units.

During the Spring, 1976, the group deployed personnel to the first Team Spirit exercise in Korea. Team Spirit exercises provide the military forces of the United States and the Republic of Korea experience in joint/combined defense operations. Since then 201 CCGP has participated annually in this exercise and has gained the respect of the Active Force whereby the group was designated the lead Combat Communications unit for 1984 and 1985. An exception was made this year while the Group stayed home to work two major gaining command inspections.

On March 1, 1976, the 201st Combat Communications Squadron was federally recognized and activated at Hilo under the command of Maj. David F. Howard. In addition, the 202 Air Traffic Control Flight was relocated to PMRF Barking Sands, Kauai and was commanded by Maj. James Kurita.

In November 1977, two additional units of the 201st were relocated. The 201st Combat Communications Flight was moved to Maui under the Command of Major Dave Rodrigues, and the 201 Air Traffic Control Flight moved to Kealahou, Hawaii where they shared facilities with the Army National Guard. Maj. Hon Ping Chee commanded this unit.

One of the purposes for moving these units was to increase the recruiting base; however, initial projections did not pan out in Kona and the 201 Air Traffic Control Flight was relocated once again to their current home at NAS Barbers Point on Oahu in 1981 where the unit has been successful in every endeavor.

On Oct. 1, 1980, Lt. Col. Ernest T. Shima assumed command of the group from the retiring Colonel Goya and Maj. Melvin C. Morris became the air technician detachment commander shortly thereafter.

Community support has always been strong. For example, during Hurricane Iwa warning, all of our units were notified and placed on alert. The Kauai unit members kept a skeleton crew on duty that evening at the Barking Sands facility that was directly in the path of the Hurricane. After braving the storm, TSgt. Clifford Tsuyama was able to establish electrical power and MSgt. Edgar Takabayashi and TSgt. Albert Antonio were able to get the HF/SSB radio on the air with MSgt. Francis DeCosta at the mike. Our contact with the 202 ATCF the morning after Hurricane Iwa was the first and only radio contacts with the devastated Island of Kauai. The full-time staff of all units as well as numerous traditional Guardsmen spring into action with equipment and manpower to provide vitally needed mobile electric power generators to run large water supply pumps on Kauai. Without these generators, Kauai county would have been without drinking water for several days.

The 201 CCGP has been involved in numerous training exercises through its history. Exercises such as Team Spirit, Balikatan, Cope North, Sea Spirit, Sentre Vigilance, Elf One, and Ulchi Focus Lens are just a few examples which have provided meaningful training and travel to such places like Korea, Philippines, Japan, Wisconsin, Diego Garcia, Italy,

Pago Pago, American Samoa, and Saudi Arabia making the 201 CCGP one of the more interesting missions in the Hawaii Air Guard today.

On July 1, 1985, the 201st Combat Communications Group was redesignated the 201st Combat Informations Systems Group. Similar changes were made in the Squadron and the two Communications Flights. However, this name change was short-lived. On Oct. 1, 1986 the name was changed once again to Combat Communications.

During this past year, the entire Group was faced with two major inspections conducted by the Air Force Communications Command. A Management Effectiveness Inspection (MEI) was conducted in March, with the Headquarters and the 201 ATCF receiving the highest rating in its history. In fact, all units did well in the inspection. Celebrating was short-lived, however, as an Operational Readiness Inspection (ORI) loomed over the horizon.

During the July-August time frame, the Air Force Communications Command conducted a two week Operations Readiness Inspection. The entire Group received high praise for their performance.

As mentioned in the beginning, the 201 CCGP has a short but interesting history. We have gone through numerous changes and have grown to be one of the best Combat Communication units in the country. We are proud of our past accomplishments and look forward to the challenges which lie ahead. You can be sure of one thing, the 201 CCGP will always strive to build pride in our unit and State.



The 201st arrives! at Team Spirit Exercise in Korea.



Looking forward to their return to sunny Hawaii, SSgt. Lorenzo P. Ruiz, SSgt. Raymond N. Kodani, SSgt. Wallace K. Kakemoto, Sgt. Ralph M. Yamada, and MSgt. Maximo P. Valenzuela disassemble and crate the TRC-97 antenna at Sachon, Korea, during Team Spirit '78.

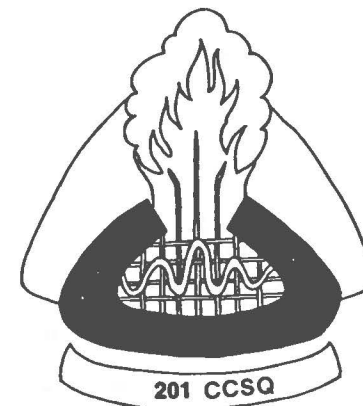


MSgt. Roy Yamasaki, maintenance officer Capt. Gwen Leidemann, and SMSgt. Clement Oshiro take care of business in the 201 CMBTCG NCMO tent. Everything at Suwon was in tents. Team Spirit '79.



Lt. Col. William N. Wright
Commander

201st Combat Communications Squadron



In 1975, in preparation for the activation of the 201 Mobile Communications Squadron (MCS) in Hilo, the parent 201 MCS (to become the 201 Mobile Communications Group) periodically sent 6-8 personnel to Hilo on a Unit Training Assembly (UTA) basis to perform maintenance on prepositioned communication gear and to prepare facilities to accept the new unit. Included in this "pioneer" group were Matt Thornley, Melvin Masuda, and Clyde Yasuhara.

This same year, most of the then 201 MCS accomplished an ambitious deployment to the "Pineapple Island" of Lanai for annual training. Unit personnel still reminisce over that event, and many hope for a "return engagement". Upon completion of the annual training, all the equipment destined for the new squadron was transported by barge to Hilo awaiting unit activation.

In 1976, the 201st Combat Communications Squadron (201 CCSQ) (then 201st Mobile Communications Squadron) relocated to the Keaukaha Military Reservation, Hilo, Hawaii. The nucleus of this new squadron included eight Air Technical personnel commanded by Maj. David Howard. Of the original Air Tech crew, SMSgt. Mel Masuda, MSgt. Ed Enovejas, and CMSgt. Clem Oshiro, are still "holding the fort". The young unit took up "temporary" residence in some World War II buildings belonging to the Army National Guard. Shortly after this, the designation of all Mobile Comm units was changed to Combat Communications, the implications for future "Total Force" involvement being evident to the thoughtful.

The following year brought additional busy taskings. Twenty-one squadron personnel participated in a seven-day exercise at the Pacific Missile Range Facility, Barking Sands, Kauai. Moses Henderson became the first 201st Team Spirit participant when he deployed to Korea as an augmentee.

Not long after that, the eruption of Kilauea Volcano prompted Governor George Ariyoshi to place members of the 201st Combat Communications Squadron on state active duty to assist Civil Defense personnel in providing communications and other assistance in the evacuation of residents of the Kalapana area. This was the first opportunity for the young unit to demonstrate its state mission capabilities to the residents of the Big Island. From the very beginning, the 201st Squadron has been known as a "Can Do" outfit.

In 1978, the 201st CCSQ began to take part in what developed as a continuing unit participation in a series of annual deployments to the Team Spirit exercises in Korea. This year the 201 CCSQ supported the Tactical Air Base (TAB) at Sachon, Korea. Learned from that experience was the fact that Korea is COLD in the winter! In future exercises the squadron made sure that such things as parkas and tent liners went with them. Many unit members have participated

in every Team Spirit exercise since, and consider this the highlight of their Guard activities. That same year, the unit's first on-island deployment was undertaken with equipment and personnel being transported to South Point via road convoy. The experience and expertise of the unit continued to grow.

The next year, 1979, saw repeated Team Spirit participation for the 201st CCSQ where most unit personnel served at Suwon. Becoming familiar with the people of Korea, some unit members began to get more involved, leading to off-duty help and assistance to a local orphanage, beginning another tradition that was to continue and expand as the years passed. The natural friendliness and helpfulness that mark the people of Hawaii are very evident here. The year was highlighted by the approval by Congress of \$1.2 million for the construction of an automotive equipment shop and \$1.6 million for an electronics maintenance facility.

Planning and design of the new facilities continued through 1980. Along with the normal activities, the unit loaned comm equipment to the project at Enewetok Atoll to assist in the cleanup of nuclear debris from the atomic tests of the 50's. The third year of participation in Team Spirit saw the squadron at Cheong Ju, Korea. The squadron continued its growth, both in strength and in experience.

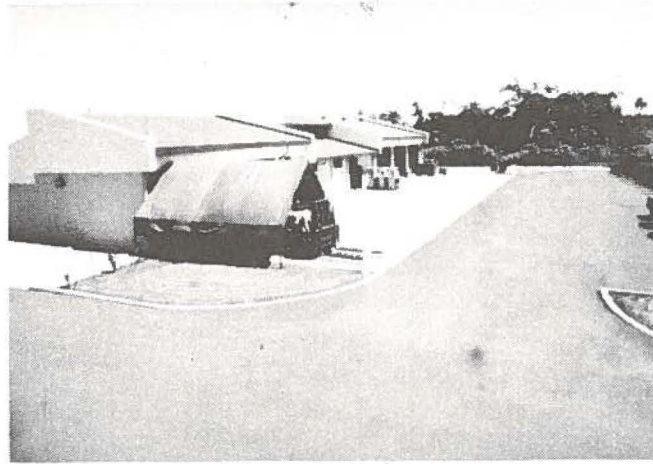
In 1981, groundbreaking ceremonies for the new facilities were held at Keaukaha Military Reservation. In attendance were State Adjutant General, Maj. Gen. Valentine Siefermann, Brig. Gen. Arthur Ishimoto, then Commander of the Hawaii Air National Guard, State Senator Dante Carpenter, State Representative Andy Levin and Mr. Howard Mimaki, the Governor's Liaison. This was a landmark day for the 201st CCSQ. Unit personnel began to get excited in anticipation of their new facilities.

Continued participation in Team Spirit Exercises saw another contingent of 201st CCSQ personnel in Korea, this time again at Sachon. Later that year, the squadron reluctantly said farewell to Lt. Col. Howard as he left the 201st for an assignment with Headquarters Air Force Communications Command (AFCC). Command of the squadron was assumed by Capt. Richard Nishimura.

In Oct., Nov., and Dec., the unit, along with the rest of the 201 CCGP, underwent a performance evaluation.

The following year proved to be another busy one for members of the 201st CCSQ. Besides continued participation in Team Spirit '82, the unit was now moving into its new home. Everyone was excited about the move, but concerned about the fact that they had to prepare for the upcoming Management Effectiveness Inspection (MEI), scheduled at the end of the year. In the final analysis, the move was made and the MEI was successfully passed.

One of the more memorable events of 1982 saw the



201st CCSQ Maintenance facility at Hilo.

100,000th person to be recruited into the Hawaii Air National Guard, Airman Basic Michael Burnham, become a member of the 201st CCSQ.

Hurricane Iwa ground past our state that year leaving the island of Kauai a tangled mess of electrical wires and debris. Most of the island was without power. Members of the 201st CCSQ including SMSgt. Mel Masuda, TSgt. Russell Flemming, SSgt. Hiram Keliikoa, and SRA Elmer Lum, raced to the aid of Garden Island residents, providing generator support until electrical systems could be restored. This type of duty was further proof of the worth of the Guard to the residents of our state in time of crisis.

Continuing the support of the active forces, the unit again participated in Team Spirit '83. Some of the unit members also participated in exercise Opportune Journey, held in Hawaii, by providing communications support to close air support aircraft flying missions at Pohakuloa Training Area (PTA) in conjunction with US Army exercises.

During this year, another first for the squadron was the selection of Sgt. Harvey Motomura as the first enlisted person from the squadron to be a candidate for commissioning through the ANG Academy of Military Science (AMS) program. Sgt. Motomura attended AMS at McGhee-Tyson AB, Tennessee and upon completion of the Basic Officer Training Program, was commissioned a 2nd Lieutenant. Lt. Motomura was assigned to the Administrative officer position upon his return to the unit.

Also in 1983, volunteer members of the unit supported the March of Dimes Walk America by providing personnel at various checkpoints throughout the course of the walkathon. This is but one example of the spirit and support that unit members give to their community. A High Frequency (HF) radio package was sent to Diego Garcia in the Indian Ocean for yet another exercise. In charge of that package was SSgt. George Allen.

The year 1984 brought change and continued growth. The unit gained a new commander when Maj. Nishimura returned to civilian employment and traditional guardsman status and was replaced by Maj. William Wright. Maj. Wright came from the 149th CCSQ where he served as commander.

During that year, the unit's strength exceeded 90% for the first time. At the annual HANG Awards Banquet, the squadron received an award for its noteworthy ground safety performance, a feat which had been achieved for several years running, and is indicative of the dedication and professionalism of the unit personnel.

Also this year, members of the 201st CCSQ participated in a Cope North exercise in Japan, the first of what will probably be an ongoing series, and Balikatan, an exercise in the Philippines.

In 1985, the squadron again demonstrated their abilities with another Team Spirit deployment. As in past years, members of the squadron donated some of their spare time

during the deployment at the Sung Yok Orphanage where they give not only their time, but money, food and clothing as well.

In June 1985, a unit deployment to Kona was the first of several exercises preparatory to the Operational Readiness Inspection (ORI) scheduled for July '86. This exercise was a mix of pluses and minuses. The deployment site was on the sunny Kona side (+), in a very small, cramped area (-), next to the ocean (+), and also next to the lava field surrounding the Keahole Airport (-). It was hot (-), the food was good (+), and the exercise went fairly well (+/-). All in all the consensus was "there's got to be a better way!"

Of particular note this year was the achievement of 100% of authorized strength for the first time in the month of July. For the second year in a row, the 201st CCSQ recruiter, TSgt. George Sensano, was named "GSU Recruiter of the Year".

The 201st took part in two separate Cope North exercises this year in Japan. The scheduled ORI in '86 would preclude participation in that year, but exercise planners look for the 201st in the following years.

During 1986, a tough February deployment to the Pohakuloa Training Area was another of the preparations for the ORI. In light of the experiences from the previous deployment to Kona, the consensus on this exercise was "this ain't it!"

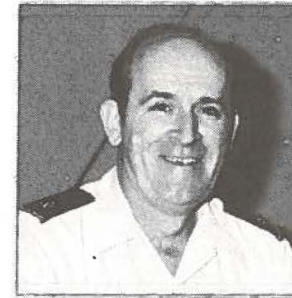
The unit followed this demanding activity by achieving good marks in the Management Effectiveness Inspection conducted by Hq AFCC in March '86. The level of performance and dedication of squadron personnel in achieving the good results were exemplified by the likes of SSgts. Lazaro Joaquin and Don Miyamoto who were recognized by the AFCC IG Team as "Professional Performers".

The last of the major activities in preparation for the ORI was the deployment to Upolu Airport in May. After a tremendous amount of planning and coordination, this activity was conducted as a comprehensive last "fine tuning" of our abilities. Many things were learned, including the consensus that "this must be it!", in comparison to our previous "garden spot?" deployment sites.

In July and August '86 "WE DID IT!" The combined efforts and talents of the 201st CCSQ and the 202nd ATCF paid off as the personnel assigned to "TAB 3" passed the ORI. Once again the hard work, dedication, and sacrifices of our personnel coupled with the "Can Do" spirit came through with flying colors. The AFCC Inspection Team again identified "Professional Performers". This time SSgt. Marge Bartlet and Sgt. Cynthia Tagaca were singled out for the honors. They were typical of the professional dedication and ability of all of our personnel as they met a very "real world" challenge, and persevered.



MSgt. B. Pabingwit during Team Spirit '84.

Maj. Louis S. Macknik
Commander

201st Combat Communications Flight



Dedication:

The late MSgt. Frank Farley was one of the 201st Combat Communications Flight's original technicians who served until his retirement in 1982. MSgt. Farley was taken ill just prior to his retirement and passed away shortly thereafter. MSgt. Farley had a great impact on our flight's early development and it's members. Those who knew him remember him with fondness and always in high regard. The 201st Combat Communications Flight is proud to dedicate our unit history to his memory.

The 201st Combat Communications Flight (201 CCFT) was federally recognized on Oct. 13, 1967. The unit was activated as the 201st Mobile Communications Flight (201 MCF), and it's first home was at Battery Selfridge at Hickam AFB. The 201 MCF was collocated with the other flights and the squadron. Initial training consisted of classroom training because the prime communications equipment had not been received.

The 201 CCFT relocated to Wailuku, Maui as part of a master plan to decentralize the 201st Combat Communications Group. The flight initially shared facilities with the Hawaii Army National Guard (HARNG). The majority of the unit was located in the basement of the HARNG's Wailuku Armory, where it always leaked when it rained. The power production/refrigeration, and wideband workcenters worked out of the HARNG's Kahului Armory. The motor vehicle workcenter was collocated with the HARNG motor pool at Paukualo, where the salt spray caused constant corrosion problems.

Through the sharing of facilities for six years, a close working relationship has developed between the Army and Air Guard on Maui which continues today. A great debt of gratitude goes to the Hawaii Army National Guard for sharing their facilities in these first lean years.

The flight conducted its first unit training assembly (UTA) at the Wailuku Armory with 30 personnel attending. Maj. David Rodrigues was in command, with Capt. Stephen Berg as the operations officer, Capt. Lance Okihara as the maintenance officer. Personnel who attend the first UTA and who are still members of the flight include: Maj. Lance Okihara, MSgt. Francis Quitozal; TSgts. Vincent Dacanay and David Kalani; SSgts. Melvyn Tabata, Edward Koja, Everett Ferreira, Robert Morisaki, and Barry Chung; and Sgt. Patrick Kina.

The initial technician manning was small but has expanded through the years. The flight's original technician crew included: MSgt. Myles Brown, TSgts. Dennis Gabriel and Frank Farley, and SSgt. Mark Tokunaga.

During our first two years on Maui, our unit training assemblies (UTAs) consisted mainly of ancillary training until our prime equipment arrived and was made operational. Hands-on training began as equipment arrived; most equipment arrived in poor shape and required extensive repairs.

Maj. Jeffery H. Okazaki assumed command from Maj. Rodrigues in February 1979. Maj. Rodrigues left to take the logistics officer position at Headquarters, Hawaii Air National Guard, a position which he still holds today.

Our first deployment occurred in February 1979, when we deployed to the Kula Community Center area. A wideband shot back to the Kahului Armory was completed using the TRC-61, TSC-62 and MGC-2/6 vans. The first summer camp was held in August, including a deployment to the Kula Community Center.

In October 1979, the first "Home and Home" with the 201st CCSQ was held in Hilo, Hawaii. This event brings both units together for a weekend of fellowship and sports competition. The sports completions held during the "Home and Home" determine the Eastern Division representative to the HANG softball, volleyball, and basketball tournaments. This fellowship has become an annual affair with the location alternating between Maui and Hilo. In 1986, the "Home and Home" was renamed the Frank Farley Friendship Festival.

Our unit's continuing participating in the Team Spirit exercises began in 1979. Sgt. (now MSgt.) Francis "Marino" Quitizol was the sole unit member deploying to Korea. The next year, 18 flight personnel deployed to Team Spirit 80. Unit participation has increased in each of the following years.

The 201 CCFT has always been active in the community. The units co-sponsored its first SuperKids competition in February 1979, which attracted over 300 children from Maui. The unit has co-sponsored this event annually since then. This and other community activities resulted in the flight being awarded the Air Force Communications Command (AFCC) Civic Achievement Award in November 1980. This award recognizes units for their continuing support of the local community.

Our first Air Force technical advisor was MSgt. John Kidney, who arrived on Maui in 1980. MSgt. Kidney served with the unit until his retirement in February 1984. MSgt. Kidney still resides in Lahaina area, and has begun a second career as a county police officer. TSgt. Mark Kadekaru, our current advisor, was assigned shortly after MSgt. Kidney's retirement.

Maj. Okazaki relinquished command to Maj. Charles V. Darby in April 1982, to assume the operations staff officer position at the Group Headquarters.

On Sept. 23, 1982, Maj. Gen. Arthur Ishimoto, along with

other dignitaries, participated in the ground breaking ceremony for the flight's new facilities. Construction was completed in December 1983. The hard work of moving from old to new was shared by technicians and traditional Guardsmen alike. On Feb. 11, 1984, the flight held its dedication and grand opening ceremonies. Among the attendees were Congressman Daniel Akaka, Maj. Gen. Alexis Lum, and Brig. Gen. Edward Richardson. The day long activities were culminated with a grand luau held at the new facilities.

In 1984, the flight was assigned as the lead combat communications unit for Team Spirit 84. More than 40 unit members deployed to Choeng Ju AB, Korea, with Maj. Louis MacKnik as the communications site commander.

To recognize deserving unit members, the flight initiated its awards program in 1984. Annually, an awards committee selects winners in five categories for personnel and workcenters which have consistently performed in an outstanding manner throughout the year. The first year's award winners included: MSgt. Alva Frias, Technician Supervisor of the Year; SSgt. Scott Coffin, Traditional Supervisor of the Year; SrA Evelyn Brito, Airman of the Year; TSgt. Gary Brito, Community Service Award; and vehicle maintenance, Workcenter of the Year.

Speaking of awards, the 201 CCFT received the National Safety Council's Award of Commendation the last four years (1982-1985) for its outstanding safety record. The flight was presented the "First Lady's Award" for outstanding community support four times (1980, 1983, 1984, and 1985). Individually, SMSgt. David Leynes received the First Lady's Outstanding Volunteer Award for Maui in 1980.

The flight had a busy year in 1986. It began with a seven day operation readiness evaluation (ORE) in February. This was

the first of two OREs planned in preparation for the operational readiness inspection (ORI) scheduled for July. During the ORE, many events went as planned and while others indicated the need for improvement. But before those items could be worked on, there was a management effectiveness inspection (MEI) to pass.

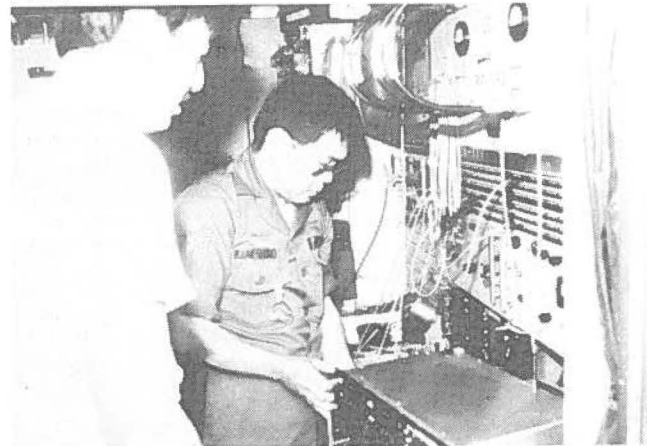
The AFCC inspection team arrived in March to conduct the MEI. Hard work and long hours by unit members, both traditional and technician, proved to be the difference as the unit improved its rating from the last MEI.

On May 1, 1986, Maj. Louis S. MacKnik, the current commander, assumed command of the flight from Maj. Darby. There was little time for celebration as the year's second seven day ORE was set for May. Again the setting was the Ulupalakua Ranch, 6000 feet above sea level. Many things had been reviewed and improved since February, but "fine tuning" was required. The ORE was interrupted by a island-wide tsunami watch. Unit personnel immediately started preparing trucks and generators for possible community support. Luckily, there was no tsunami. With the two OREs completed, the real thing, the ORI, loomed over the horizon.

After two years of planning and practicing, the AFCC Inspector General team arrived to conduct the ORI in late July. The inspection ran for eleven days, running through the initial response, employment, combat support, and ability to survive and operate phases. The "can do" attitude of the flight members and augmentation personnel proved the difference as the unit passed its first ORI. Three flight members were recognized by the AFCC Inspector General as "Professional Performers" for their work during the inspection. They were: TSgt. Vincent Dacanay (operations superintendent), TSgt. Walter Pacheco (ground radio maintenance technician), and SSgt. Mitchell Maeda (communications center supervisor).



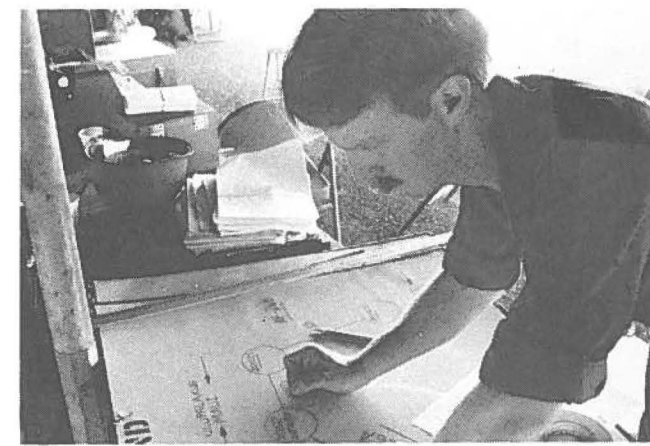
201st CCFT electronic maintenance facility at Kahului, Maui.



SSgt. Kaneshiro troubleshooting the control head for the AN/TSC-60.



SSgt. Marino Quitazol, Capt. Jeffrey Okazaki and MSgt. Frank Farley, early in the history of the flight.

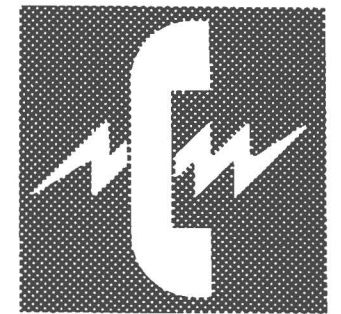


SSgt. Doolin plots frequency usage during a 201st CMBTCG field deployment.



Lt. Col. Jeffery H. Okazaki
Commander

202nd Combat Communications Flight



Although the 202d Combat Communications Flight (CCFT) is comparatively young in the 40 year history of the Hawaii Air National Guard, the 202 CCFT is already established with a legacy of excellence. Our "roots" go back to the 109th Aircraft Control and Warning Squadron which later evolved into the 201st Mobile Communications Squadron (MCS) on Mar. 6, 1967.

The 202d Mobile Communications Flight (MCF) was federally recognized on Oct. 13, 1967 as a subordinate unit of the 201 MCS. A reorganization from mobile communications to combat communications on Apr. 1, 1976 and subsequent name changes has brought the 202 CCFT to its present form as one of five operational units under the 201st Combat Communications Group (CCGP), Hawaii Air National Guard, headquartered at Hickam AFB, Hawaii.

The federal mission of the 202 CCFT is to provide the Air Force with communications-electronics services for tactical air forces and to support emergency U.S. Air Force requirements for communications facilities. To accomplish this mission, it is authorized 146 military members, 16 of whom are full-time air technicians.

The flight was originally located under austere conditions in Battery Selfridge at Hickam AFB. Extensive self-help projects led by men, such as Walter Furuyama, Galen Nakahara, Paul Mori, Donald Higa, Roy Ichimura, and Walter Nishihara transformed the facility into useable work areas.

In 1970, the 202 CCFT moved to its present location at Building 3382, Hickam AFB. A new mobility warehouse was completed in August 1986 to support the unit's mobility mission.

The 202 CCFT is commanded by Lt. Col. Jeffrey H. Okazaki, who assumed the position in August 1984. The list of 202 CCFT commanders began with Maj. Ernest T. Shima from October 1967 to June 1979. Maj. Shima was succeeded by Maj. Roy A. Matsumura from July 1979 to August 1983 and Lt. Col. Paul S. Mori from September 1983 to July 1984.

The 202 CCFT maintains its high state of readiness through constant training. Both personnel and equipment are exercised to ensure timely support of any worldwide mission for which the unit may be called. Training takes place on Oahu as well as at various locations throughout the state and the world. Members of the 202 CCFT have participated in many Joint Chiefs of Staff (JCS) exercises, including Team Spirit, Cope North, Focus Lens, and Balikpapan.

The Flight's active support of Air Force missions dates back to 1969 with unit participation in Southeast Asia Treaty Organization (SEATO) Exercise "Sea Spirit" in Thailand and

the deployment of unit personnel to Pago Pago, American Samoa, to assist recovery forces for Apollo space missions.

The 202 CCFT has participated in every Team Spirit exercise in the Republic of Korea since 1977. During this period, the flight has served as the lead unit on three occasions, most

recently in 1985. Highlights of Team Spirit '85 included a pioneering success in the transition from the quick reaction package (QRP) to heavy 407L communication equipment. Another achievement was the first successful High Frequency Defense Communication System (HF DCS) entry exercise from a bare base in Korea to Japan and the Philippines. The 202 CCFT's lead role contributed directly to these achievements.

Another pace-setting accomplishment by the 202 CCFT, is its successful HF DCS entry exercise program. Since May 1984, the Flight has successfully participated in nine HF DCS exercises, which consist of passing secure record communications traffic into the DCS (AUTODIN).

The unit also stands ready to perform its state mission to protect life and property in the event of disaster. In the aftermath of Hurricane Iwa, which struck Kauai in 1982, the 202 CCFT provided emergency generator support and numerous citations were given to unit members for their humanitarian efforts.

During peacetime and times of state emergency, the 202 CCFT reports to the Commander, 201 CCGP; the Commander, Hawaii Air National Guard; The Adjutant General, Hawaii; and the Governor.

When mobilized, the 202 CCFT reports to the Air Force Communications Command (AFCC) through the Pacific Information Systems Division (PACISD), Hickam AFB, Hawaii.

The 202 CCFT is actively involved in the local community and it lends its support to various activities designed to better the quality of life for everyone. Among those charitable activities is the unit's participation in Hickam AFB's Youth Festival Carnival, Friends of the Library book drive, and sponsorship of a senior citizen's home and the Sung Yook Orphanage in Korea.

Among its numerous awards, the 202 CCFT has recently received the prestigious ANG Mission Support Plaque for FY 84-85, the National Safety Council Award of Commendation for the fifth consecutive year, and the HANG Outstanding Unit Award for 1985.

The 202 CCFT has proven itself to be an important part of the Hawaii Air National Guard and a valuable asset to the U.S. Air Force under its Total Force policy.



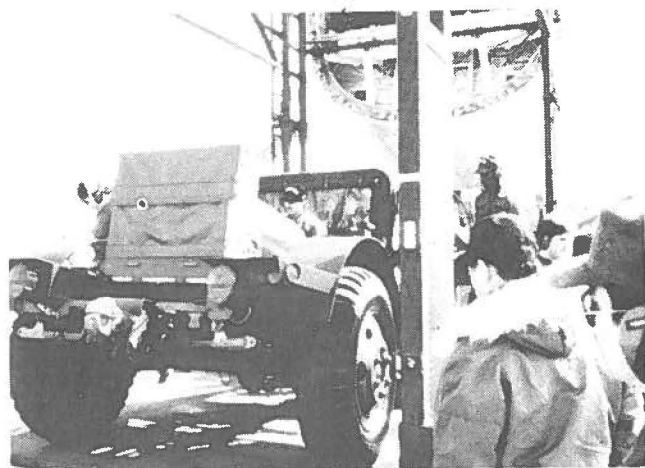
AN/TRC-97 shot from Dillingham Air Field to PFMF during 1969 comm exercise.



SrA Cody S. Nakasone, 202 CMBTCF, HANG, hammers antenna tie-down stakes during the installation of the communications operations at Suwon. Team Spirit '79.



MSgt. Jim Roland, right, the PCA advisor to the 201st CMBTCG, coordinates the installation of communications equipment at Suwon Air Base, Korea, with two Hawaii ANG members—Amn. Gladys Rosenbush (left) and SrA Cody S. Nakasone (right) of the 202 CMBTCF during Team Spirit '79.



A frequency converter is off-loaded from a CRAF 747 at Osan AB, Korea, during Team Spirit '83.



MSgt. Frank Johnson. His was the loudest "yo" at roll call. The sharpest uniform in formation. The warmest smile in greeting. We miss him. . .

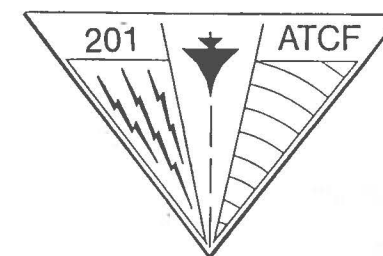


TSgt. Alama supervises MDC operations at Team Spirit.



Maj. Russell K.N. Mun
Commander

201st Air Traffic Control Flight



This year (1986) is HANG's 40th Birthday and is also a year of special historical significance to the 201 Air Traffic Control Flight (ATCF). First of all, it is the 20th anniversary of the unit's formation and federal recognition. Second, it marks 20 exciting years of change, organizational development and tremendous growth for the unit.

Though the 201st ATCF has been in existence since 1967, it is really one of the youngest members of the HANG family because of its recent location to NAS Barbers Pt. Being the "new kid" on the block has not always been easy. The HANG standard of excellence is a tough tradition to live up to. At times the challenges have been overwhelming for our fledgling unit, especially when we've been too eager to start running before we've learned to walk.

In retrospect, however, the past 20 years have been full of excitement, challenge and learning for all unit members. The unit's many accomplishments have served to establish an excellent foundation for continued growth and should encourage us to continue to strive to do better in the future.

The mission of the 201st ATCF is to provide terminal air traffic control (ATC) services for tactical air forces. This mission involves the rapid deployment of personnel and mobile ATC equipment to support air traffic control operations anywhere in the world. This worldwide commitment requires the capability to mobilize, deploy and operate our equipment under adverse field conditions.

The men and women of the 201st are dedicated to the accomplishment of our mission. Our primary goal is to provide the best terminal air traffic control services possible and to successfully support any contingency USAF requirement for air traffic control.

On October 13, 1976 the 201st ATCF was federally recognized as a subordinate unit of the 201 Mobile Communications Squadron. The unit was located at Battery Selfridge, Hickam AFB awaiting construction of its new facilities across the street. Maj. Roger Watanabe was appointed the unit's first commander. In 1972 Maj. Hon Ping Chee succeeded Maj. Watanabe to become the unit's second commander.

In 1976 under the 201st Mobile Communications Squadron's reorganization plan, the squadron's mission was expanded with the formation of the Group Headquarters and its geographically separated units. The 201 ATCF was programmed for relocation to Kona, HI.

For FY 1977, the 201st stayed in a holding pattern awaiting its pending relocation to Kona, HI. It was attached to the Group Headquarters for training purposes and allocated temporary quarters at the 202nd Combat Communications Flight (CMBTCF) Bldg. at Hickam AFB. It was authorized only one air technician and assigned a limited number of military personnel.

The physical relocation of the unit to Kona began in 1978. The move served to relieve the overcrowded training condi-

tions associated with the collocation at Hickam and supplied new opportunities for recruiting and training on the neighbor island. It also dispersed valuable military assets and made available additional contingency forces to the State of Hawaii in the event of natural disaster.

The year 1978 was a year full of pioneering efforts by the unit. Lt. Col. Chee and Capt. Blaine Izuka, the newly hired detachment commander, along with a small band of air technicians began the formidable task of establishing the unit to Kona.

After securing joint use agreements with the Army National Guard for Kealahoukeua Armory, and permission from the FAA to use portions of Keahole Airport, arrangements were made to ship the unit's equipment and supplies to Kona.

In Oct. 1978, the unit participated in a joint exercise to South Point with the 201 CMBTCS, Hilo. The Tower was packed, mobilized and convoyed South Point for a five day deployment. Capt. Izuka, MSgts. Roy Hidano and Toshio Nagai and SSgt. Roy Paniku spearheaded the successful deployment.

Controllers got a chance to train with other Guard members from around the country in August of that year during deployment to Alpena ANGB, MI. USAF and Air Guard instructors conducted excellent tower, GCA and maintenance support training. The highlight of the deployment was a Hawaiian style luau sponsored by the Hawaii Guardsmen with the traditional pineapples, ti leaves and music brought over from Hawaii.

The unit exercised its State mission in 1979. Due to a severe drought in Waimea, the unit was tasked by State Civil Defense to supply two MB-18 generators and personnel to pump water from lower elevation reservoirs to storage tanks in the affected area. TSgt. Cliff Dalen and AIC John Ako worked many long hours providing vitally needed service to the stricken area.

The following year marked the unit's first participation in the JCS Exercise, Team Spirit, SMSgt. Robert Kamiyama, MSgt. Nagai, TSgt. Dalen, TSgt. Calvin Tateishi, SSgt. Albert Ancheta, SSgt. William Kurashige and SSgt. Morris Pabing-wit represented the unit in Korea. These 201st ATCF members were presented a plaque from the Korean Air Force for contributions to the success of the exercise.

In 1980, Lt. Col. Chee retired from HANG after some 22+ years of dedicated military service. Capt. Russell Mun was appointed the new unit Commander in April.

Summer camp in 1980 emphasized mobility training for all personnel. This involved equipment packaging, motor vehicle training, convoy procedures, tent erection, perimeter defense and chemical warfare training.

In 1980, the unit participated in two service projects. 201st members helped with the Kona Special Olympics for handicapped/retarded children by providing time keepers and

game marshalls for athletic events. In addition, approximately 15-20 unit members volunteered to help the Keala-kehe School PTA. Using a lot of hard work and sweat, they helped level and seed the school's new playground.

Fiscal Year 1981 was a year of upheaval and change as we prepared for the relocation of the unit to Naval Air Station Barbers Point on Oahu.

At Barbers Point, the unit was housed in Bldg. #80, an old WWII quonset hut that was scheduled for demolition. But with the help of the HANG Civil Engineers and \$100,000 from NGB, Bldg. #80 was refurbished and now serves as the present home of the 201 ATCF.

The renovation was completed in 1982 with the expansion of the motor vehicle compound and repainting of the entire building. And in June of that year, the unit received the National Safety Council's President Award Letter for its excellent safety program. MSgt. Nagai's fine efforts as the unit's Safety NCO merited this national distinction.

In late 1982, equipment and personnel were employed to help in the Hurricane Iwa disaster relief effort. Both generators and personnel were airlifted to the island of Kauai.

Under the direction of SMSgt. Norman Uyechi and TSgt. Ron Matsumoto various smaller deployments such as Focus Lens (Korea), Cope North (Japan) and HARNG ALCE were supported by our radio personnel.

MSgt. David Langsi, 201st ATCF First Sergeant, was selected the Outstanding HANG Senior NCO for 1984. This was in recognition of his 30+ years of dedication to duty and his superior performance in the HANG.

And to cap off the year, Capt. Izuka transferred to the 202 CCFT upon selection as the new detachment commander. This was a well-deserved promotion for him. His years of hard work and dedication were instrumental to the unit's growth.

Our sixth consecutive and largest year of participation in Team Spirit was in 1985. Capt. Bert Kumabe was appointed 201st ATCF detachment commander and chief of maintenance. Our controllers took part in the WESTPAC training program that year working alongside regular USAF controllers in places like Kadena AB, Okinawa, Osan AB, Korea

and Clark AB in the Philippines. Controllers also participated in the NGB's Sentry Vigilance Program at Volk Field, Wisconsin and Alpena ANGB, MI.

In June, the unit completed its first full blown system training exercise under the newly established Air Force Communications Command Operational Readiness Inspection (AFCC ORI) criteria. This involved the air mobility, road convoy, deployment, combat support and survivability phases of the ORI scenario.

The next month, the unit's softball team won a trophy and raised almost \$700 in contributions in the 6th Annual Budweiser Easter Seals Softball Marathon. And in November, at the Annual Awards Banquet, TSgt. Moses Akana was awarded the HANG Outstanding ATC Controller Award, the first-ever recipient.

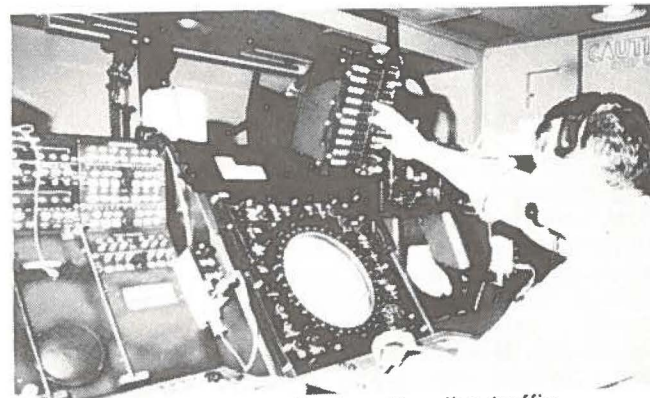
This year, 1986, is already shaping up to be a busy year full of important activities for the unit. It started off with a visit from Maj. Gen. John B. Conaway, who toured our facilities and met with our key personnel in January.

During the March AFCC Management Effectiveness Inspection (MEI), the unit received an exceptionally fine rating and numerous commendable comments from the inspection team. The areas of ATC Operations, Communications maintenance/logistics, and Command/leadership were rated "excellent."

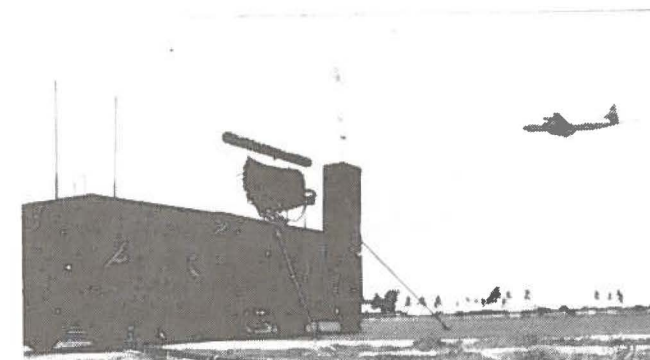
We also broke ground this year for construction of our \$2.7 million, 3 acre new facility at Barbers Point. It also caps the final installation of our MPN-14 RADAR and the eventual use of this facility to provide "live" training and ATC services to Runway 11 at Barbers Point.

The 201st ATCF celebrates its 20th birthday with as much pride as HANG celebrates its 40th Anniversary. Our 20 years of existence finds the unit with over 100% manning and with almost all the HANG Management indicator goals being achieved. We are working diligently to increase our readiness rating and should make substantial gains in 1987.

The 201st ATCF is proud to be a part of the Hawaii Air National Guard family and is striving to uphold the HANG tradition of excellence. The new kid on the block is coming of age and is ready to run with the best of them.



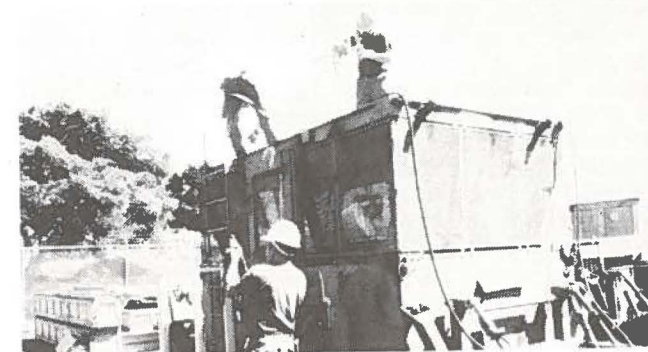
Radar Controller handling live traffic.



AN/MPU-14 on the ready line.



ATC personnel install wind speed/direction indicators for AN/TSW-7.



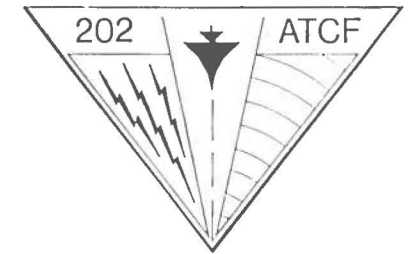
AN/TSW-7 being prepared for deployment.



Capt. Barry J. Castellano

Commander

202nd Air Traffic Control Flight



The conversion of the 201st Mobility Communications Squadron to the 201st Mobility Communications Group in 1976 also birthed a new Kauai unit to the Combat Communications family—the 202nd Air Traffic Control Flight (ATCF). This 59 person unit was formed to do basically two missions. First to provide terminal air traffic control services for the USAF as national security requires and secondly, to defend the State of Hawaii and its people from mass violence, originating from within human or natural causes. With such a challenging charter the 202nd experienced birthing pains during the early times of recruiting and growing to fill the manpower requirements with qualified individuals. We soon discovered that air traffic controller prospects do not grow on trees.

The first full time crew consisted of one administrative troop, then SSgt. Harold Uyeda and TSgt. Clifford Tsuyama of power production. Operating out of the old Kekaha Armory, which was jointly shared with the 150th Aircraft Control & Warning (AC&W) Squadron, the unit slowly started to fill out. Maj. Charles Prentiss was the first Commander. He laid the early groundwork for moving the unit to a newer, permanent facility, a place to call home. Maj. James Kurita took over in 1978 the year of the groundbreaking for the new Communications/Electronics Training Facility and combined Automotive Maintenance, Aerospace Ground Equipment Shop/Mobility Storage Facility at Barking Sands, Pacific Missile Range Facility. This was the beginning of the 202 ATCF as we know it today. It was at this time that we adopted the emblem patch, a brainstorm of the technician force, and MSgt. Francis Takara in particular. Depicted on an inverted triangle are lighting bolts symbolizing ground-to-air communications and radar waves to symbolize our radar and air traffic control services. The fighter aircraft on final landing approach illustrates the successful result of the two functions working together.

In 1980, Major Kurita retired. He will always be remembered as the concerned, involved commander who "bought the coffee" on UTA weekends. He stayed on long enough to see the unit finally move into its new site in June 1980, a culmination of many long hours of hard work and planning. And who can forget our "brown shoes" guest speaker, Maj. Gen John Guice. It was a very special event.

Assuming command from Major Kurita was Maj. Vincent Nishina. Famous for his cooking skills during Princeville airport deployments, "Vince" stressed the mobility training aspect of our mission. Under him, the unit held tower and Ground Control Approach (GCA) camps every year. It was not only an opportunity for people to work or "show off" but also to promote comradery. The close unit family atmosphere would pay off later on.

One of the greatest challenges that Major Nishina and the 202nd faced was responding to the Hurricane Iwa disaster

which decimated parts of Kauai on November 23, 1982. The 202nd would play a key part in island restoration actions. An emergency High Frequency (HF) link was established with the 201 CCGP, Hickam AFB, which temporarily served as the primary means of interisland communications. Generators from the unit were loaned to the Board of Water Supply to keep the pumps going, and to the Kauai Veterans Memorial Hospital for emergency treatment of Hurricane victims. MSgt. Francis DeCosta, the air traffic control technician manned the Barking Sands tower while Navy helicopters ferried relief crews back and forth from the 150th AC&W Squadron at Kokee AFS. His diligence in the face of personal danger was instrumental in keeping Kokee's operation going uninterrupted and weighed heavily toward his selection as the HANG Senior NCO of the Year for 1983.

Hurricane Iwa, more than any other event during the unit's brief history, graphically illustrated the 202nd's "can do" attitude and guardsmen spirit. But there were many other contributions by the unit on the community as well as military level that were not as highly publicized.

In 1979, volunteers from the 202nd participated in the first SUPERKIDS event ever sponsored on the island of Kauai. These dedicated individuals provided the manpower to make the American Lung Association sponsored program a success.

A deployment to Alpena, Michigan was also in the works in the year 1979. Extensive training for the air traffic controllers in a fixed tower and training in an MPN-14 Ground Control Approach (GCA) for the controllers and maintenance personnel provided an enriching experience for our personnel. Ground radio maintenance personnel also participated in the training environment.

Members of the 202nd ATCF participated in their first of many Team Spirit Exercises in 1979 along with other members of the 201st Combat Communications Group. This is an annual Joint Chiefs of Staff (JCS) sponsored exercise held in the Republic of Korea. Both the Alpena and Team Spirit deployments were great opportunities for our people to interact with fellow guardsmen, foreign nationals, and the active forces. They were extremely rewarding deployments. 1979 was also a big year for individual accomplishments. SSgt. Ernest Delacruz was selected as the HANG NCO of the year for 1978. But the true accomplishment is one that results in the preservation of life. A1C Benjamin Palacio came to the aid of an elderly woman who was choking. He applied the abdominal thrust maneuver for air obstruction and was able to dislodge the food after several attempts. A1C Palacio comforted the 71-year-old tourist until an ambulance arrived. The training in the 202nd was a vital factor in saving a valuable life. A1C Palacio was a true hero in every sense of the word.

In 1980, the HANG Motor Vehicle Inspection Team rated

the 202nd the best in HANG, thanks to the efforts of MSgt. Harvey Maeda. Also, MSgt. Edgar Takabayashi provided a vital HF communication link between the 150 AC&W Sq. and the 326 Air Division (AD) during a violent storm which generated winds up to 60 knots, severely damaging and degrading communications between the air defense centers. He established an emergency HF network, making it possible for the 150th to continue its operational mission uninterrupted.

In the training area, AB Kent Yamauchi, an air traffic controller, was an Honor Graduate for his technical school class at Keesler AFB, MS. A proud precedent for 202nd controllers. Participation in Vagabound Warrior 1981 was a first time achievement for the 202 ATCF in supporting the PACOM and CONUS based forces field training exercise. Members participated in providing equipment and air traffic control support to aid the smooth accomplishment of the exercise.

Also, the 202nd participated in the PMRF Armed Forces Day Open House/Air Show and supported a fine open house by the Navy. The unit provided a mobile control tower display to expose our mission and unit to the civilian community.

1981 was the initial year of Chemical Warfare (CW) training for the personnel of the 202nd who undertook the training with great enthusiasm. The mobility seriousness of the mission really hit home. Several personnel responded to the call to duty and participated in Team Spirit 81 in Korea. Once again the personnel proved themselves to be valuable in the accomplishment of the mission.

On the training side of the house, A1C Dean Agustin received the "Fast Burner Certificate" for his excellence in Air Traffic Control training by phasing ahead of his peers in the academic portion of training at Keesler AFB Training Center. Our first graduate of the Community College of the Air Force program was TSgt. Alvin T. Burley, our Air Traffic Controller technician. He graduated in April 1981.

Besides Hurricane Iwa, 1982 was an intense busy for the 202nd. Several aspects of our mission capabilities were severely tested.

The TSW-7 was required by the host base, PMRF, to support its airfield during the renovation of its fixed tower site. The period of operation was from May through June 1982. The tower proved to be an invaluable asset to Naval operations during this timeframe. Although not manned by 202nd personnel, the 202nd was proud to press one of its equipment into service for our Naval friends.

Cope Canine 82 was another exercise supported by the Radio Operations personnel of the 202 ATCF.

The 202nd participated in the Federal Cheese distribution for the State of Hawaii. Personnel were utilized to transport the cheese to distribution centers. Our proud personnel were happy to be able to support the needy members of our community.

A Presidential's Award Letter for Safety was received by the 202nd in July 1982 for its safety record as a unit.

1983 was a year of several noteworthy accomplishments:

- The 202nd supported the Hawaii Health Fair 83.
- Unit members took part again in Team Spirit 83.
- Volunteers from the 202nd ATCF supported the local Boy Scouts Council by providing assistance for cleanup at Camp Allen Faye. The aftermath of Hurricane Iwa left many trees and fallen debris on the road to the Boy Scouts area.

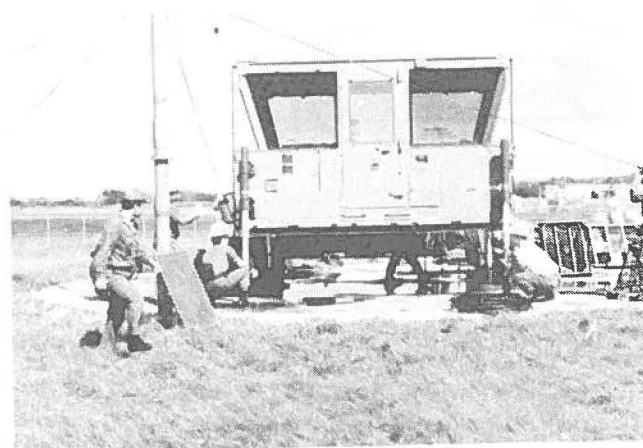
A National Safety Council President's Award Letter was received by the 202 ATCF for 1983. In 1984, the unit supported the Health Fair 84, Team Spirit 84, and again received a President's National Safety Council Award for zero ground mishaps.

In August 1985, Lt.Col. Vincent Nishina retired as the Commander of the 202nd ATCF and was aptly replaced by Capt. Barry Castellano in October 1985. This was the new beginning of another decade under a new Commander with high ideals.

Immediately confronting the new Captain were the AFCC MEI and ORI. Through the teamwork and closeness as

mentioned earlier, the unit passed both impressively. TSgt. Lowell Agena and SSgt. Douglas Brandt were named Professional Performers. The unit worked long and hard, but through all the tension, people could look back and laugh at particular incidents, like the convoy getting lost in the dunes of Pohakuloa for four hours, or SSgt. Byron Wong "shot" by his own supervisor during the ORI, or SMSgt. Takabayashi and his MEI PMEL "stash". By going through the tough times together, team character and cohesiveness were developed like no other means possible.

Reflecting back on the short but proud history of the 202nd and keeping our two basic missions in mind, one can assert that we can surely do our job. We've had several occasions to display our talents and in all cases our talented people came through. We are proud of the accomplishments of the past and look forward to the many challenges of the future.



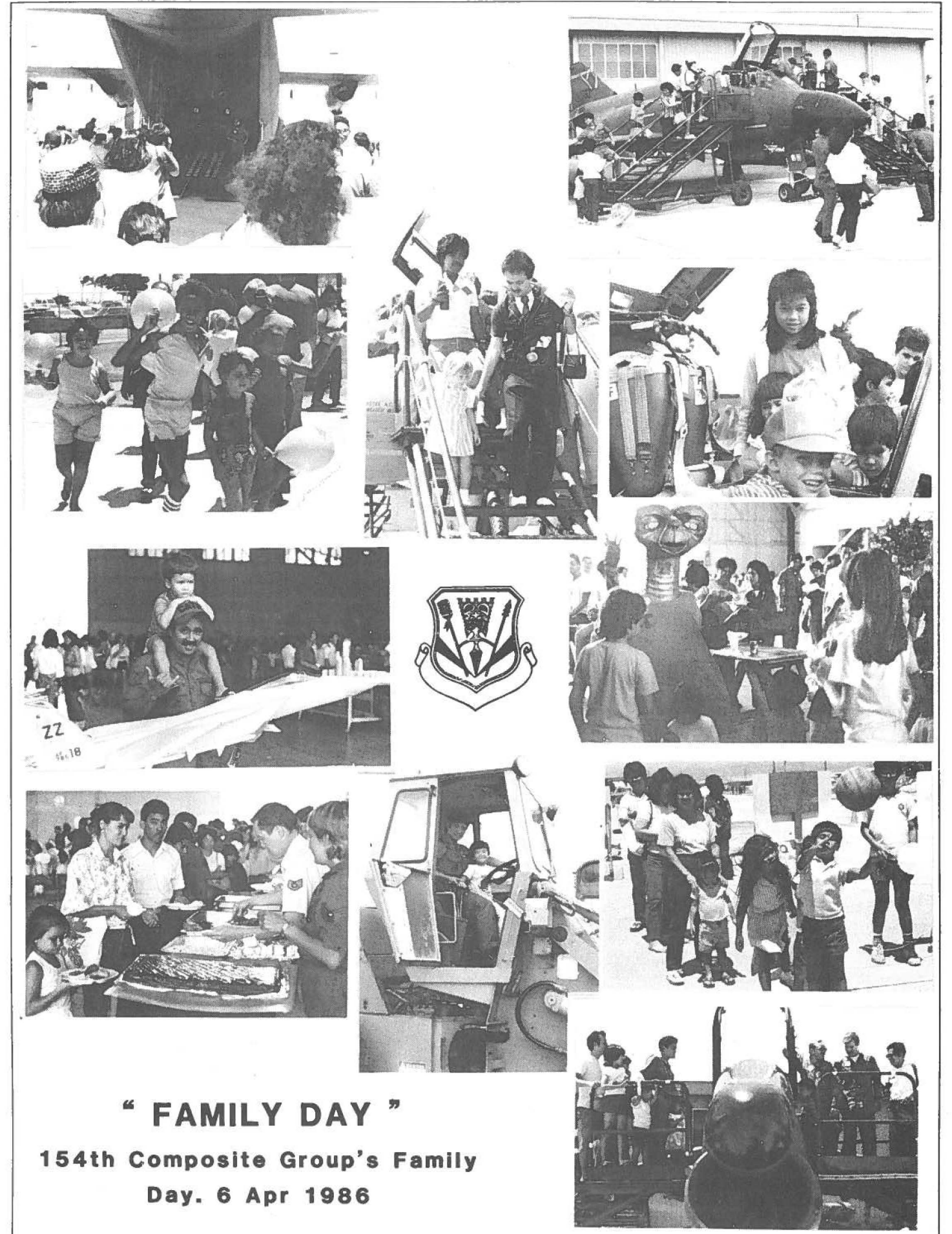
Workers set up a TSW-7 Mobile Control Tower, used to control air traffic, especially in remote undeveloped areas.



Personnel assisting Friends of the Library



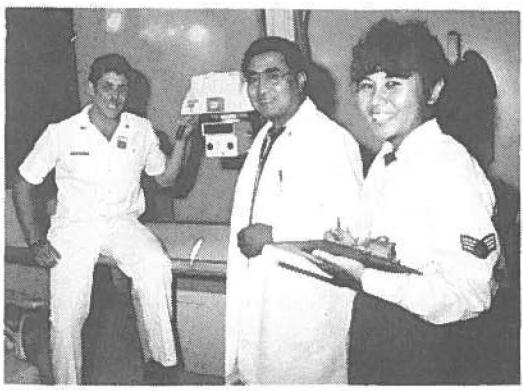
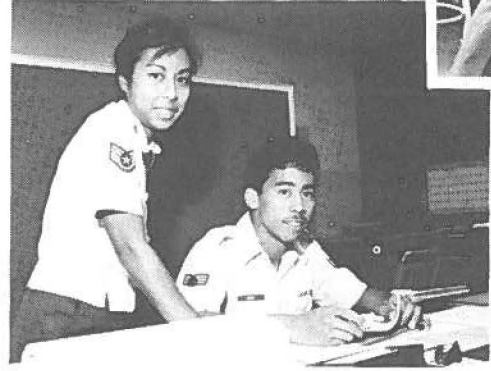
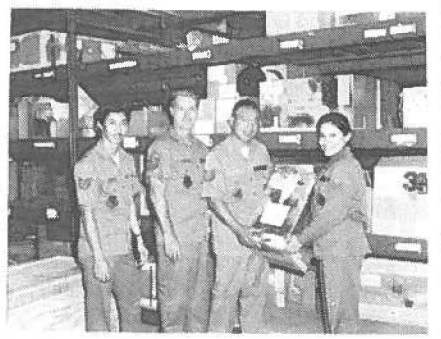
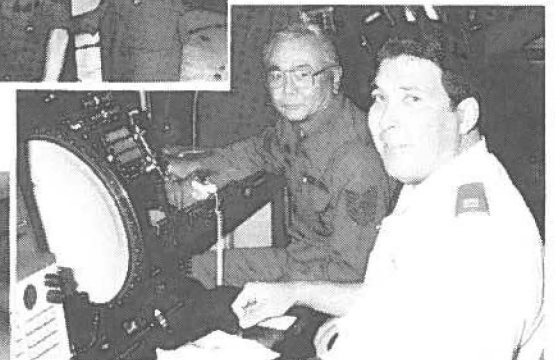
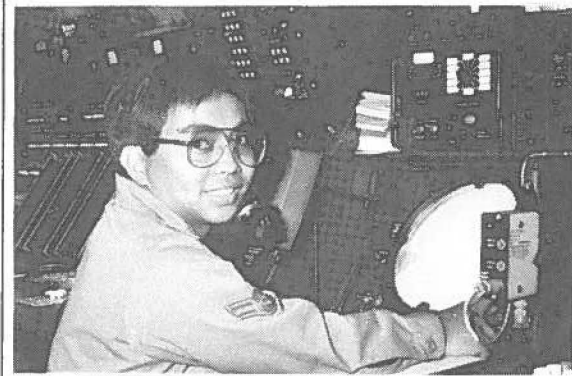
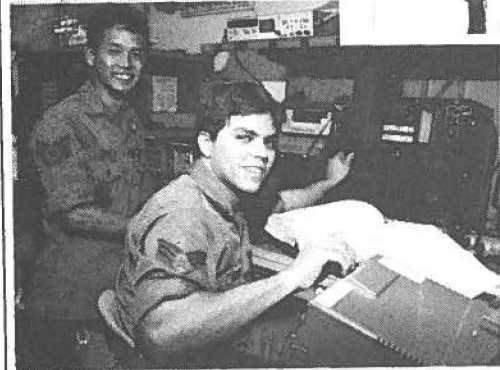
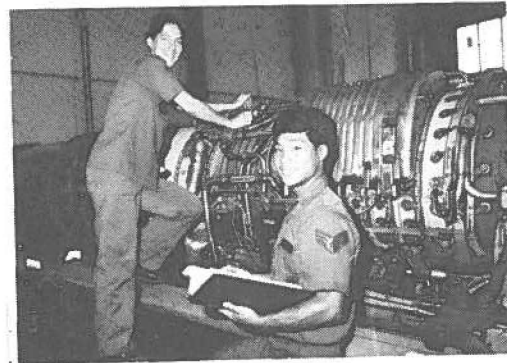
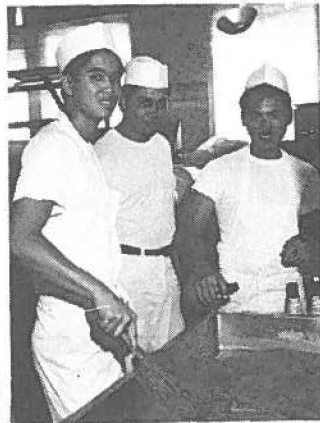
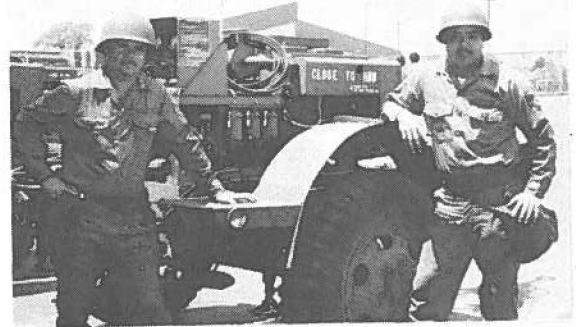
Amn. Saligumba controls aircraft via radar at Barking Sands.



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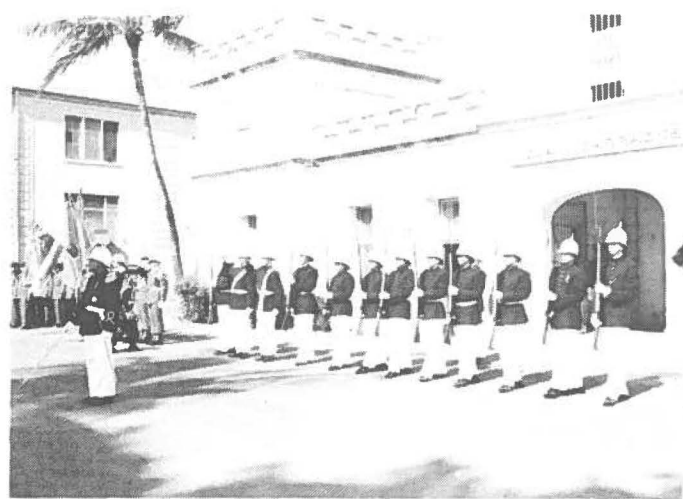
PEOPLE PROGRAMS



The NCO Academy Graduates Association was organized in 1968. Membership is open to graduates from an accredited noncommissioned officer academy who are current members of the HANG or retired members of the armed forces. Hawaii's organization is known as Chapter 18. The association was organized to perpetuate the best traditions of the noncommissioned officer corps of the Air National Guard of the United States; to promote and maintain morale, discipline and esprit de corps; and to encourage a feeling of goodwill and mutual understanding among the noncommissioned corps.

Members of Chapter 18 are annually involved in diversity of community projects and activities which they sponsor or support. Some of these community projects include Business and Industry Day to promote employer support for the Guard; the Easter Seal Telethon; Special Olympics; Cancer Crusade for the American Cancer Society, an on-going clothing drive and annual Halloween Party for the Waimano Training School and Hospital.

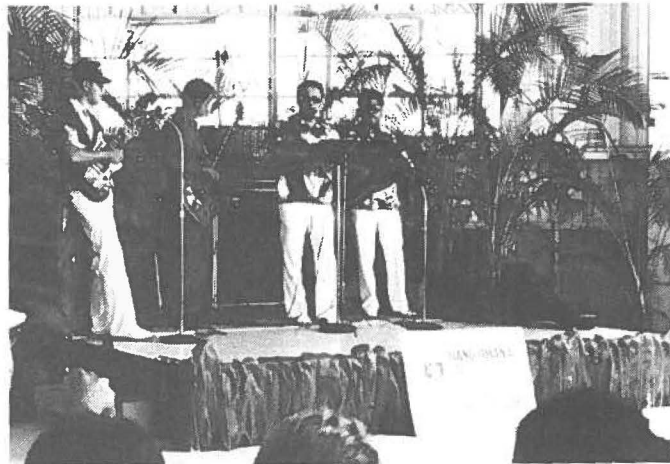
ROYAL GUARD



ROYAL GUARD.

On Nov. 16, 1963, the HANG Royal Guard was formed and composed of Air Guardsmen of Hawaiian ancestry. They are uniformed and equipped in similar fashion to the Royal Guard of King Kalakaua to perpetuate a portion of Hawaiian traditions. The HANG Royal Guard participates in numerous civic and military functions each year and has had the honor of performing in the 1965, 1973, and 1977 Presidential inaugural parades. These and other such events in which they have participated have been a source of pride for all of

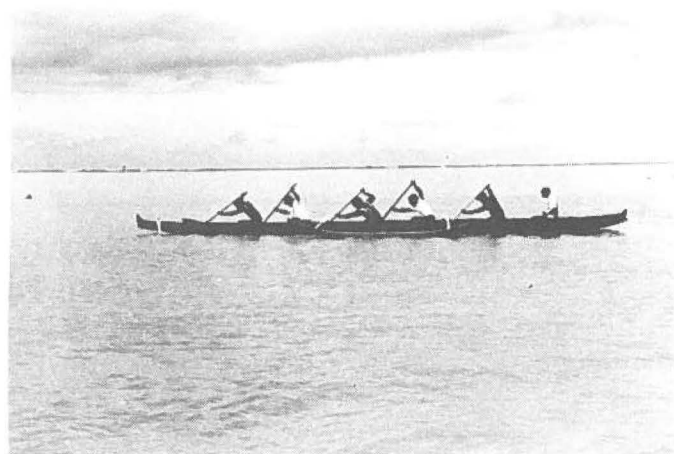
HANG OHANA



HANG O'HANA.

The HANG O'HANA was formed as a non-profit corporation in 1977 for the purpose of promoting the ALOHA SPIRIT within the Hawaii Air National Guard. It is composed of three clubs each with their own members and organizational structure.

The Music and Hula Clubs (more properly NA MELE KOA and NA NANI O WAHINE KOA respectively) are also comprised of individual HANG members with a multiplicity of job specialties totally unrelated to their musical and dancing skills. They are voluntarily in these clubs and often spend much of their own personal time practicing and performing for various events which build a closer bond between the HANG and the community.



HANG CANOE CLUB.

The Canoe Club was established for fellowship and exercise. It is open to all guard members willing to endure a rigorous workout in the waters off the Sea Breeze Restaurant at Hickam. Members enjoy the opportunity to participate in many community sponsored races throughout the year.

The HANG O'HANA welcomes any Air National Guard member desiring to donate some time, talent, and ALOHA to this very worthwhile organization.

HANG ATHLETICS

The HANG Athletic Program offers opportunities for all HANG members to participate in recreational sporting events to stimulate, develop and maintain personal, mental, physical and social well being. The program also develops esprit de corps and team leadership abilities. Members of HANG are encouraged to participate in scheduled tournaments of basketball, softball, volleyball, golf and tennis. There are also annual marksmanship competitions of pistol and rifle matches, air rifle matches and a Goodwill Fishing tournament sponsored by the 150 ACW Sq, Kauai.

SOFTBALL

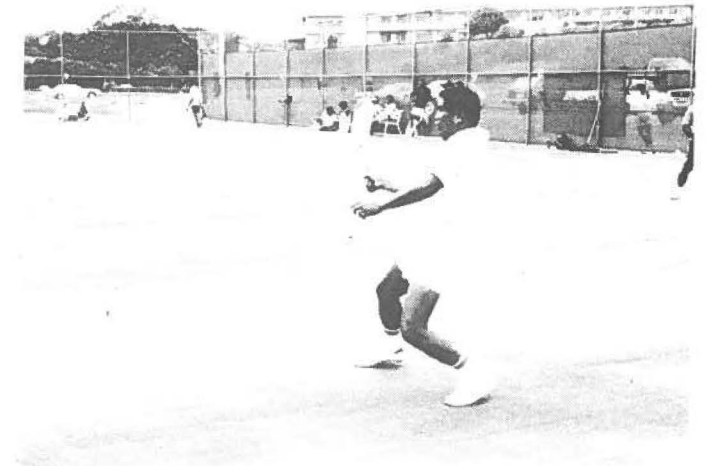


BASKETBALL



HANG'S TEAM SPIRIT: "GEEV 'UM!
HANG 1986

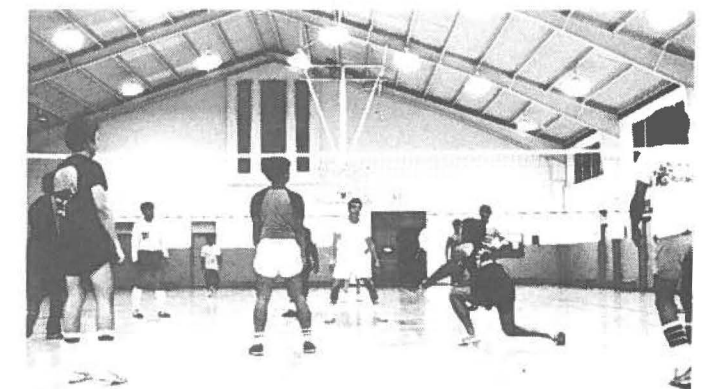
TENNIS



GOLF



VOLLEYBALL



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In the Line of Duty

Lt. Leopold J. Brady — Aug. 1, 1949
Maj. Carlos W. Bonham — June 15, 1952
Lt. Andrew S. Whitaker — Sept. 19, 1953
Lt. Ira B. Wynne — Mar. 17, 1954
SSgt. Peter M. Kaanapu — June 8, 1955
SSgt. Arthur B. Fitzsimmons — July 18, 1955
Lt. Clinton S. Ballentyne — June 14, 1956
Lt. Charles F. Johnston — June 21, 1963
Maj. James Y. Sato — Jan. 7, 1964
Capt. Dean E. Hedlund, Jr. — Nov. 23, 1967

While members of HANG

MSgt. Patrick K. Honda
A1C David A. Kaanaana
A1C Herbert Y. Isa
SSgt. Theodore K. Bonite
A1C Roland K.K. Pang
SMSgt. George K. Meyer III
TSgt. Guy M. Matsuguma
MSgt. Frank Johnson
A1C March Ambo
AMN Jeffrey DeRego
MSgt. Frank Farley
TSgt. Eric Kanazawa
MSgt. Gordon Martin
SSgt. Lawrence Shiroma
TSgt. Brian Nishikawa
SSgt. Van Kunukau
Maj. John Oshiro
SSgt. Gerald Kaaa
A1C Joseph Nobriga

Retirees

Lt. Col. William F. Dau
SMSgt. William K. Enoke
SSgt. Herbert Panoke
MSgt. George Joaquin
TSgt. Lawrence S. Partika
TSgt. Samuel N. Kekaula
MSgt. Frederick H. Rittmeister
MSgt. George G. Wela
MSgt. Adam Y. Aki
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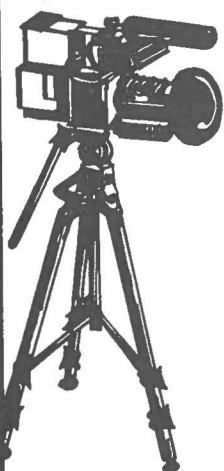
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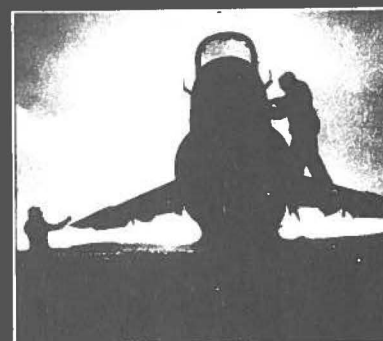


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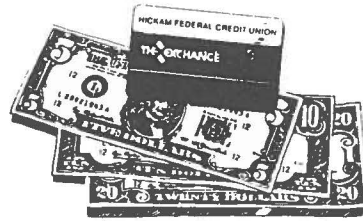
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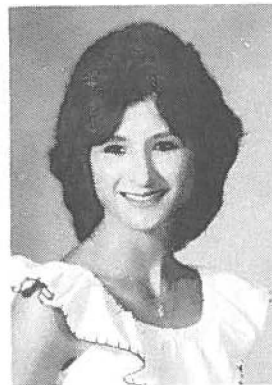


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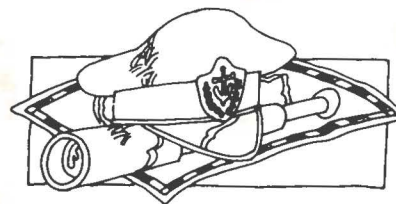
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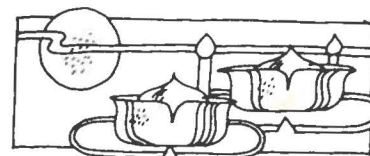


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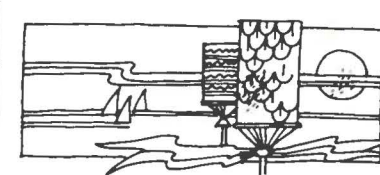
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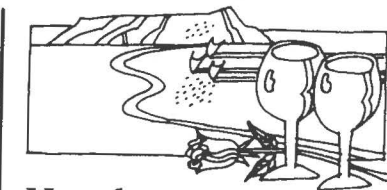
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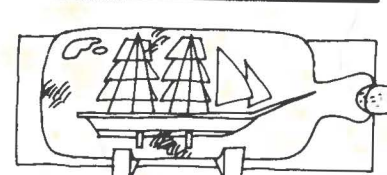
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